



MEMORANDUM

Colutor

TO: Tim Baroody, City Manager
FROM: Mike Craig, Zoning Administrator
DATE: November 16, 2016 for the November 22 meeting
SUBJECT: RZ2016-03 – Hamptons at Family, L.P. (Contract Purchaser) requests a rezoning of GPIN 7769-77-8378 and a portion of GPIN 7769-87-3295 (the “Property” totaling 20.840 acres) from R2, Residential to Commercial Highway (CH) and R12, Residential (R12).

ISSUE

Should the City Council rezone 20.84 acres from R2 to CH and R12 to enable the development of an automotive sales establishment, 78 townhomes, and 120 multi-family dwellings?

RECOMMENDATION

Approve the rezoning in accordance with the proffer statement and general development plan.

UPDATED DISCUSSION FOR NOVEMBER 22 MEETING

The City Council held a public hearing on this item on October 25 at which one person spoke against this application. The speaker cited impacts on the schools. After this discussion, which included topics such as commercial and residential phasing, school capacity, density, housing affordability and sustainability, City Council tabled the item until the November 8th meeting.

On November 8th City Council again discussed the above matters, and reacted to the amended proffers submitted on November 3, 2016. After further discussions with the applicant, staff offers more clarity on questions related to affordability, the financing being pursued by the applicant and the amended proffers. After this information below and on the following two pages, the balance of this Council package is unchanged from the November 8th version.

The Applicant has stated their intention to use Low Income Housing Tax Credit (LIHTC) funding for the proposed 120 multi-family unit portion of the project. The program uses tax credits to incentivize the development of housing units that are affordable to households who make 60% of area median income (AMI) or less annually. The AMI is based on the Washington D.C. Metropolitan Statistical Area, which means that HUD calculates Fredericksburg’s AMI as \$108,600. Census data reports the median household income for the City of Fredericksburg at \$49,454.

The LIHTC program establishes maximum tenant income limits set at 60% AMI. The top chart on the page at the end of this section compares the tenant income limit (based on household size and

number of bedrooms) to different full time worker salaries in the City of Fredericksburg. The salaries are range lines for full time entry level positions to senior positions.

The LIHTC program assigns maximum rents calculated based on an affordability standard where rent (plus a utility allowance) does not exceed 30% of that bracket's household income. The rents are calculated by bedroom and are as follows:

Unit Type	Monthly Rent
1 Bedroom	\$1,050
2 Bedroom	\$1,285
3 Bedroom	\$1,400

The maximum rents listed above would exceed 30% of the income levels of entry level public sector employees, entry to mid-level nurses, and most retail employees. To better address affordability for people making comparable salaries to these types of workers, the Applicant has now proposed to proffer to rent 12 units (6 one bedroom and 6 two bedrooms) at a 50% AMI income limit at the applicable 50% AMI maximum rent for 5 years. The bottom chart on the next page compares the 50% tenant income limit (based on household size and number of bedrooms) to the same full time worker salaries shown in the top chart. The rents for the 12 proffered units would be:

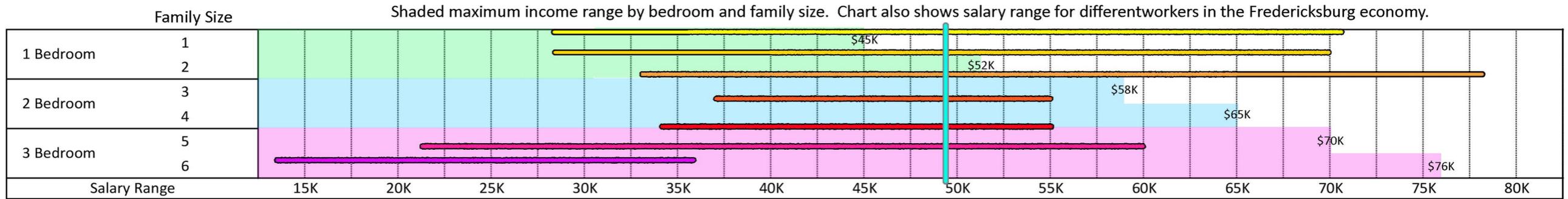
Unit Type	Monthly Rent
1 Bedroom	\$897
2 Bedroom	\$1,066

The effect of this proffer is that families working within the Fredericksburg economy (including full time employees at the police department, fire station, or in the schools as well as people working in the retail core in Central Park and the medical campus around Mary Washington Hospital) would be able to afford to live, on their own, within the City. To rent units of this quality in other complexes would likely place these households in housing burden situation.

The Applicant had also proffered \$100,000 to be used by the City to implement the goals and policies related to affordable housing in the City's Comprehensive Plan and the City's adopted Affordable Housing Policy. The City Attorney has determined that until the City adopts an affordable proffer zoning ordinance in accordance with § 15.2-3305 of the Code of Virginia, the City cannot accept cash proffers for affordable housing. The Applicant has provided a letter stating its intention to work with the City as a side agreement to provide the previously proffered \$100,000 for affordable housing initiatives under the City's Housing Advisory Committee.

Finally, the revisions to the proffer statement do not constitute a material change to the project nor do they result in the rezoning of the property to a more intensive zoning classification. These proffer changes may be accepted by the City Council without referral to the Planning Commission.

The two charts referenced above are on the following page. Again, with the exception of the section of this report related to housing affordability, the remainder of this report is unchanged from the November 8th version of the Council package.



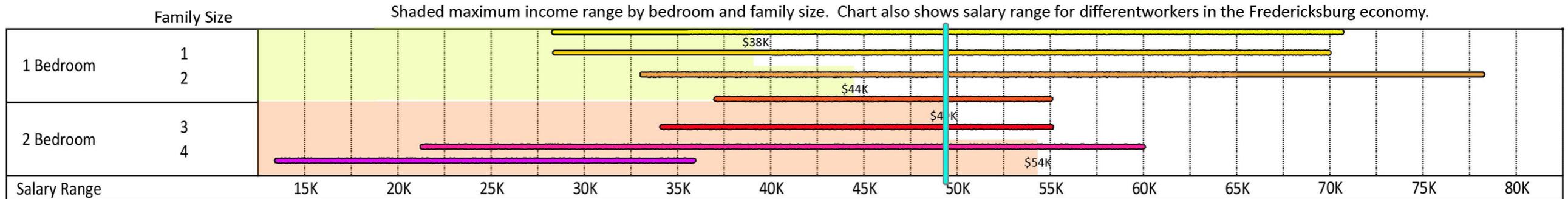
* Income limit data from the Federal Department of Housing and Urban Development.
 City of Fredericksburg median household income taken from census data.
 Salary range for Fredericksburg public sector workers provided by the City of Fredericksburg.
 Private sector salaries provided by the Applicant.

Median household income
 City of Fredericksburg

KEY

60% AMI	1 BR Income Qualification
	2 BR Income Qualification
	3 BR Income Qualification
	Police administrative staff and officers
	Fire administrative staff and officers
	School administrative staff and teachers
	LPN
	Admin. Assistant II
	Retail Manager
	Retail Clerk

MAX SALARY VS. WORKER SALARY RANGE
 STANDARD MARKET RATE UNIT (60% AMI INCOME LIMITS)
 APPLICABLE TO 108 UNITS FOR FIVE YEARS THEN ALL 120 UNITS



* Income limit data from the Federal Department of Housing and Urban Development.
 City of Fredericksburg median household income taken from census data.
 Salary range for Fredericksburg public sector workers provided by the City of Fredericksburg.
 Private sector salaries provided by the Applicant.

Median household income
 City of Fredericksburg

KEY

50% AMI	1 BR Income Qualification
	2 BR Income Qualification
	Police administrative staff and officers
	Fire administrative staff and officers
	School administrative staff and teachers
	LPN
	Admin. Assistant II
	Retail Manager
	Retail Clerk

MAX SALARY VS. WORKER SALARY RANGE
 PROFFERED AFFORDABLE UNITS (50% AMI INCOME LIMITS)
 APPLICABLE TO 12 UNITS FOR FIVE YEARS

CITY COUNCIL PUBLIC HEARING AND DISCUSSION (NOVEMBER 8 MEETING)

The City Council held a public hearing on this item on October 25 at which one person spoke against this application. The speaker cited impacts on the schools. After discussion, the City Council tabled the item until the November 8 meeting.

Discussion of this item focused on five topics – commercial and residential phasing, school capacity, density, housing affordability and sustainability:

1. Commercial and Residential Phasing –

Council members asked if any phasing of the development was to be proffered. The concern was whether or not the commercial development was speculative at this time. The applicant stated that no phasing was proffered; that the construction of the multi-family housing would begin in about two years, and that the construction of the single family attached would commence at about the same time. The applicant stated that there was a strong market for the automobile sales commercial use. The City has received a letter of support for this application from Clay Huber, President of Huber Motor Cars, expressing his interest in building a third dealership on the Hamptons at Coleman site. The letter is attached to this memo.

No previous rezoning in the City has included a proffer that commercial land use be constructed prior to residential land uses. Commercial and residential phasing was discussed with the 2015 Mill District rezoning but, due to ownership issues and the specific use mix of the proposed Germania Mills building, was not included in the final approval.

The City has approved commercial land uses that revert to residential use. In the Idlewild (2003) rezoning to Planned Development – Residential (PDR), the applicant proffered that they would build Live Work Units (mixed use townhomes) around a central plaza in the project. The Applicant included a proffer that the Live Work Units were required to be marketed and sold as commercial for five years after which they could be converted to townhomes. The Live Work Units were built and still function as such.

Two previous rezonings included residential phasing:

- a) In the Riverwalk (1991) rezoning to R-4, the applicant proffered an initial 31 unit phase followed by a second 39 unit phase occurring three years after rezoning.
- b) In the Idlewild (2003) rezoning to PDR, the applicant proffered a phasing plan limiting residential construction to 200 units per year for 4 years.

The most common use of phasing in previously approved rezonings within the City has been to address the implementation of infrastructure improvements and payment of cash proffers to different phases of the proposed development. The Hamptons at Coleman project includes the proffered phasing of infrastructure improvements and cash proffers:

- a) Prior to Occupancy of first multifamily building the Applicant shall:
 - i. Construct the Briscoe Lane Improvements;
 - ii. Construct and dedicate the Shadmoor Drive Improvements;
 - iii. Construct (or pay the City for) the extension of sidewalk to Central Park;
 - iv. FRED Stop (including bench and shelter);
 - v. Construct (or pay the City for) the crosswalk across Fall Hill Avenue at Gordon W. Shelton Boulevard;

- vi. Construct the Noyack Lane turn lane;
- vii. Construct the on-site club house and pool;
- viii. Pay the City the cash for wayside panels;
- b) Prior to Occupancy of each residential unit:
 - i. Pay the City \$5,050 per unit for schools and fire and rescue (total \$1,000,000).
- c) Prior to Occupancy of the automotive sales use:
 - i. Build the enhanced landscaping berm along Fall Hill Avenue.
- d) Upon City request:
 - i. Provide legal public access across Noyack Lane and Islip Lane.

2. School Capacity –

Another Council inquiry addressed the new students to be generated by the residential development, and the capacity of the schools to absorb these new students. The following chart was taken from a Facilities Assessment Update completed by Mosley Architects in January of 2015. The Assessment included projected school occupancy based on the existing land use entitlement through 2024 in the City of Fredericksburg:

ENROLLMENT	Base Year									
	2015	2016	2017	2018	2019	2020	2021	2022	2023	2024
Elem (PK-5)	1,821	1,891	1,954	1,962	1,985	2,038	2,087	2,125	2,161	2,222
Middle (6-8)	677	705	752	822	854	875	859	869	912	937
High (9-12)	1,038	1,066	1,101	1,101	1,130	1,116	1,212	1,276	1,287	1,319
	3,536	3,662	3,807	3,885	3,969	4,029	4,158	4,270	4,360	4,478

CAPACITY	Base Year									
	2015	2016	2017	2018	2019	2020	2021	2022	2023	2024
Elem (PK-5)	2,118	2,118	2,118	2,118	2,118	2,118	2,118	2,118	2,118	2,118
Middle (6-8)	880	880	880	880	880	880	880	880	880	880
High (9-12)	1,164	1,164	1,164	1,164	1,164	1,164	1,164	1,164	1,164	1,164
	4,162	4,162	4,162	4,162	4,162	4,162	4,162	4,162	4,162	4,162

UTILIZATION	Base Year									
	2015	2016	2017	2018	2019	2020	2021	2022	2023	2024
Elem (PK-5)	86%	89%	92%	93%	94%	96%	99%	100%	102%	105%
Middle (6-8)	77%	80%	85%	93%	97%	99%	98%	99%	104%	106%
High (9-12)	89%	92%	95%	95%	97%	96%	104%	110%	111%	113%
	85%	88%	91%	93%	95%	97%	100%	103%	105%	108%

* Source: Facility Assessment Update (Moseley Architects; K&C Associates), Jan. 2015

The 20.84 acre property is currently zoned R-2. By-right this property could be developed as 42 single family homes. According to data from the Fredericksburg Public Schools, single family homes generate 0.32 students per unit meaning that by-right this property would expect to generate 14 school age children.

The 20.84 acre property is proposed to permit 78 townhomes and 120 multi-family residential units. According to data from the Fredericksburg Public Schools, townhomes generate 0.458 students per unit and multi-family units generate 0.217 students per unit. Based on these rates, the proposed

townhome units would generate 36 students. The proposed multi-family units would generate 26 students. Thus, the proposed zoning would generate 62 total students or 48 students more than would be generated with the by-right development of the property. According to data from the Fredericksburg Public Schools, of the 48 students above by-right, 15 would attend lower elementary, 12 would attend upper elementary, 8 would attend middle school, and 13 would attend high school.

The City Council has approved three rezonings and a special use permit since 2014 that have increased the by-right number of dwelling units permitted in the City. See the discussion of public facilities in the Comprehensive Plan Compliance section on page 13 of this report for more details. The increase in entitlement would add 38 total students to the school system. Based on data from the Fredericksburg Public Schools, of the 38 additional students, 11 would attend lower elementary school, 9 would attend upper elementary school, 7 would attend middle school, and 11 would attend high school.

If the Hamptons at Coleman project was approved, the total number of students by school above the assumed projection in the 2015 Moseley study (conservatively including the 2014 Governors Row rezoning) would be 26 lower elementary students, 21 upper elementary students, 15, middle school students, and 24 high school students. As shown in the chart above, there is now capacity for these students in the school system and will be through 2020.

The City anticipates the expansion of capacity in several of the public schools as well as the construction of a new school on the Idlewild School site. The students that come out of the project would be part of the City's projected growth that will require additional capital investment in expanding the City school system. The Hamptons at Coleman project includes a \$900,010 cash proffer to offset the capital impacts of the project on the schools. The school proffer breaks down as:

- a) \$ 4,545.50 per unit.
- b) \$14,516.29 per child.
- c) \$18,750.21 per child above what is permitted by-right.

The three other rezonings approved since 2014 included \$90,000 in cash proffers to the Community Foundation of the Rappahannock River Region for the benefit of City public schools and \$170,000 in cash proffers to the City for capital impacts on the public school system. The Hamptons at Coleman project also includes \$99,990 in cash to offset impacts on public safety.

3. Density –

Council members expressed concerns about the residential density of the proposal. The proposed zoning contains 198 units on 16.53 acres for a density of 11.96 units per acre. The Comprehensive Plan's land use categories defines Medium-Density Residential as 8 units an acre or more. High-Density Residential is defined as 12 units an acre or more. By definition the proposed use would be considered under the Comprehensive Plan as Medium-Density Residential. The Planned Development – Commercial District permits up to 24 units per acre..

Here are some comparable residential densities (with their zoning in parenthesis):

- a) Hamptons at Noble (CH) – 128 MF Units on 10.97 acres @ 11.67 units per acre;
- b) The Havens (PD-C) – 232 MF Units on 16.84 acres @ 13.78 units per acre;
- c) Heritage Park (R-16) – 202 Units on 12.33 acres @ 16.38 units per acre;

- d) The Seasons (PD-C) – 250 MF Units on 13.48 acres @ 18.55 units per acre;
- e) Riverview (R-30) – 96 MF Units on 4.80 acres @ 20.00 units per acre;
- f) Cobblestone (CD)– 398 MF Units on 19.48 acres @ 20.43 units per acre;

4. Affordability –

Affordability is addressed in Comprehensive Plan Chapter 7: Residential Neighborhoods and Housing. Goal 7 of that chapter is Affordable Housing, which states that “all persons who live and work in Fredericksburg should have the opportunity to rent or purchase safe, decent, and accessible housing within their means” (pg 95). Affordable housing is specifically discussed on page 94 of the plan, which states, “the City has consistently allowed a wide range of housing types to be develop” including, specifically, apartment complexes. This project contains two types of housing, townhomes and apartments.

The City uses Community Development Block Grant funds (CDBG) to address housing affordability. The City provides Downpayment and Closing Cost assistance and Emergency Home Repair through the City’s Community Development Block Grant (CDBG) program to create and maintain affordable housing units. The City provides funding to four non-profit organizations to indirectly address housing affordability and reduce cases of homelessness. These agencies provide emergency housing grants (two agencies), legal aid, and food assistance through its CDBG program. The City provides staffing for the Continuum of Care run through the George Washington Regional Commission. Money for this staff person also comes from the City’s General Fund. One option for addressing housing affordability in the City would be to allocate more local funds to address this issue.

Other municipalities mandate that a percentage of units in a project be offered at a rate that would be affordable to households making less than the 80% of the median income of the municipality (usually with density exemptions or bonuses) or participate in other programs that directly subsidize affordable housing. U.S. Census Data states that in 2014 Fredericksburg’s median household income was \$49,454.

Council members questioned whether monthly rents beginning at \$1100 were truly “affordable” in the Fredericksburg market. Housing affordability is often described based on Census Bureau Washington Metro Area Median Income (AMI) calculations. About 20 localities are included in the U.S. Census Bureau definition of the Washington metro region, including the District of Columbia, Prince George’s County (Maryland), Alexandria, Arlington, and Fairfax County.

	AMI for a family of 4	AMI for a family of 3	AMI for a family of 2	AMI for a family of 1
Washington Region	\$108,600	\$97,740	\$86,880	\$76,020
City of Fredericksburg	\$70,150	\$63,150	\$56,150	\$49,150

The Applicant has now included two proffers aimed at addressing housing affordability in the City. Proffer 8.A states that the Applicant agrees to reduce the residential rents on 12 of the units in the Apartment project by \$200 a month for five years. Proffer 8.B states that the Applicant agrees to pay \$100,000 to the City for low income housing initiatives within 180 days of the rezoning approval.

5. Conclusion and sustainability –

One question posed by Council members was whether the proposed land uses are sustainable. The proposal is for medium density residential land use and an expansion of an existing vehicular sales node along Fall Hill Avenue. The residential portion of the project contains two housing types, two ownership opportunities, and is less dense than four out of the five closest apartment complexes to the site as well as Cobblestone. Wegmans has sent a letter of support for the project stating that the rooftops near there site are good for their business. The letter is attached to this report.

The proposal includes the continued private development of a publically accessible multi-modal transportation network between Fall Hill Avenue, Interstate 95, and Central Park. The proposal is phased so that this infrastructure network (including off-site roads, off-site pedestrian links, and an on-site bus stop) is completed prior to the occupancy of the first multi-family building.

The proposal will produce 48 students more than the on-site development currently permitted by-right. According to data from the Fredericksburg Public Schools, the additional students will not drastically accelerate the need for the City to make capital expenditures to expand school capacity. Before any students come out of the multi-family units, the developer will pay a cash proffer to offset a proportional share of the cost of expanding the City's school system.

Generally, the City has advanced affordability by approving a variety of housing and through the implementation of a CDBG program. This proposal provides a variety of housing types and ownership opportunities and now includes two elements aimed at affordability.

The Hamptons at Coleman proposal includes the development of publically accessible infrastructure and investment in public service capacity. The development of infrastructure and investment are phased to be completed prior to people moving into the apartments. The Hamptons at Coleman project could be a sustainable piece of an evolving future core in Planning Area 1 of the City of Fredericksburg.

PLANNING COMMISSION PUBLIC HEARING AND RECOMMENDATION

The Planning Commission held a public hearing on this item on August 31, 2016 at which two members of the public spoke. One speaker was concerned about the effect of the development on the rural character of a rental home he owned off Briscoe Lane. The second speaker stated that he had served on a committee several years ago to keep this area of the City green and have less intensive uses. After discussion, the Planning Commission voted to recommend approval of the project 4-3.

EXISTING USE AND CHARACTER OF THE PROPERTY

The Property is a 20.84 acre parcel located between the Volvo and Mercedes automotive sales establishments and the 128 unit Hamptons at Noble apartment project under construction to the north (zoned Commercial Highway/Conditional); Interstate 95 to the east; Briscoe Lane, farmed fields, and five single family homes and a 34 acre farm to the south west (zoned R2); and Fall Hill Avenue and Wegmans grocery store to the north west (zoned PD-C). The Property was annexed into the City in 1984.

Topographically, the Property is generally flat with very few trees and is currently a corn field. The Property has a high point in the middle which splits site drainage. In the south east corner of the site is a 0.11 acre farm pond. South of the pond is a roughly 175 linear foot stream that drains to the south and ends at Interstate 95 where it enters a culvert and flows under the roadway.

On the other side of the highpoint are 0.52 acres of non-tidal wetlands in the middle of the property as shown on the attached wetland delineation map. The wetlands are currently in the middle of the cornfield and drain to the north.

Fall Hill Avenue is the primary public road serving the project. The City is currently widening Fall Hill Avenue in the vicinity of the project from two to four lanes with a curb median. Access to the project from Fall Hill Avenue will be provided from:

- Noble Way – an existing four lane local street that will have a full movement intersection with Fall Hill Avenue once the Fall Hill Avenue project has been concluded. Public access across the Hamptons at Noble project via a private roadway aligning with and connecting to the proposed Islip Lane is being developed as part of the apartment complex currently under construction.
 - o The City anticipates installing a signal at the intersection of Noble Way and Fall Hill Avenue at a future date to be determined after the completion of the VDOT Fall Hill Avenue project. The 2015 Hamptons at Noble rezoning proffered \$91,000 towards the signalization and Celebrate Virginia South committed to paying half the cost of a signal at the intersection in a side agreement.
 - o Public access across the portion of Noble Way on the Volvo and Mercedes car dealerships (currently a private road) was proffered along with the original car dealership rezoning. An order to comply with the proffer and to permit public access across the portion of Noble Way (either by easement or through dedication of right-of-way) was sent to the owners of the property in 2015. The owners are working on documents to convey public access across the property to the City but have not completed the process yet. The owners are legally obligated to do so.
- Briscoe Lane – a two lane local street under a right-of-way easement, shoulder and ditch sections and no pavement markings.
- Noyack Lane – a proposed two lane private street with public access to be built, along with a right turn lane and taper, by the Applicant.

As part of the Fall Hill Avenue widening project, the City is building a trail on the north side of Fall Hill Avenue and a sidewalk along the south side.

Public water and sewer are available on the site.

PROPOSED ZONING MAP AMENDMENT

1. Code of Virginia:

Virginia Code § 15.2-2284 provides the criteria to consider when evaluating a zoning map amendment:

- a) “Zoning ordinances and districts shall be drawn and applied with reasonable consideration for
- b) the existing use and character of property,
- c) the comprehensive plan,
- d) the suitability of property for various uses,
- e) the trends of growth or change,
- f) the current and future requirements of the community as to land for various purposes as determined by population and economic studies and other studies,
- g) the transportation requirements of the community,
- h) the requirements for airports, housing, schools, parks, playgrounds, recreation areas and other public services,

- i) the conservation of natural resources, the preservation of flood plains, the protection of life and property from impounding structure failures,
- j) the preservation of agricultural and forestal land,
- k) the conservation of properties and their values, and
- l) the encouragement of the most appropriate use of land throughout the locality.”

2. *Proposed use:*

The 20.84 acre Property is proposed to be rezoned to two different zoning districts.

A 4.31 acre portion of the Property adjacent to Fall Hill Avenue is proposed as a commercial automotive sales establishment zoned Commercial Highway (CH), separated from Fall Hill Avenue and the vehicle sales establishment by a proposed extension of the enhanced landscaping and berm proffered and constructed with the Mercedes and Volvo establishments.

Within the 4.31 acre commercial area, the GDP currently shows a 20,300 square foot vehicle sales building, vehicle display area, and parking lot. While the GDP estimates a 0.11 Floor Area Ratio, the GDP notes that the square footage and display area proposed may vary at the time of site plan approval up to the maximum allowable Floor Area Ratio of 0.70.

A proposed public street, Shadmoor Drive, separates the project’s residential and commercial sites. A 25 foot wide Type “D” Buffer (an opaque buffer with 10 average caliper inches [ACI] of canopy trees, 13.5 ACI of understory trees, and 23 shrubs per 100 linear feet) will be located on the residential side of Shadmoor Drive between the automotive sales establishment and a proposed 16.53 acre mixed residential site proposed to be zoned R12. The R12 area consists of 198 total residential units designed around an integrated network of streets and sidewalks and integrated open space and recreational areas. The 198 units would be built at 11.96 units per acre, which is close to the maximum of 12 units per acre.

78 of the units are townhomes. The townhomes front on either public or private streets and have rear alley access for garages. A 0.36 acre open space is proposed between townhomes 13-22 and the vehicle sales use and a 0.37 acre “mews /open space” is proposed between townhomes 23-34 and Sag Harbor Lane. The townhome block between Briscoe Lane, Shadmoor Drive, and Islip Lane (consisting of townhomes 35 – 78) is proposed between existing single family homes.

A 0.53 acre central open square is proposed in the middle of the residential portion of the site between the townhomes and apartments. The green contains open space, a +/- 3,575 square foot clubhouse complex, and a pool.

The remaining 120 of the units are multi-family. These units are proposed in five three-story buildings that contain 24 units apiece. Parking for the units is either perpendicular or parallel and is adjacent to each building. A 25 foot wide Type “D” Buffer is proposed between the apartment units and the existing single family homes to the west creating a buffer where none is required by the ordinance. Access to the multi-family portion of the project is from Noyack Lane, Sag Harbor Lane, and Islip Lane.

As proposed, both the commercial and residential uses conform to the standards in Article 3 (generally, bulk regulations, setbacks, open space, and other dimensional standards), Article 4 (the use table and the use standards, specifically those found in § 72-41.2.E regarding Multi-Family

Dwellings or § 72-41.2.F regarding Townhomes), and Article 5 (generally, access, parking, utilities, landscaping, etc.).

3. Transportation:

The development provides an integrated network of complete streets in accordance with the vision and policy established in the Comprehensive Plan and Unified Development Ordinance. The development will produce a combined 2,023 vehicles per day including 144 vehicles during the a.m. peak and 196 vehicles during the p.m. peak hour. The Applicant produced a traffic analysis which showed that the new Fall Hill Avenue construction, the proposed signalization of the Noble Way and Fall Hill Avenue intersection, and the improvements proposed with this application are sufficient to handle the increase in traffic.

In regards to transportation, the proffers include:

- The upgrading of the north side of Briscoe Lane to include curbs, on-street parking, and sidewalk where adjacent to the project.
- The building and dedication of Shadmoor Drive, which is a new public road parallel to Fall Hill Avenue and will be built by the applicant through the site, as well as across right-of-way dedicated to the City in conjunction with the Hamptons at Noble project.
- The VDOT Fall Hill Avenue widening project will extend the public sidewalk to end 70 feet short of the existing sidewalk adjacent to Central Park. The Applicant either will construct the additional 70 feet or pay the City a cash proffer to build a connection in the public right-of-way.
- The construction of a FRED stop, including applicable signage and shelter, within the property.
- The construction of a crosswalk across Fall Hill Avenue at its intersection with Gordon Shelton Boulevard (essentially a connection to Wegmans).
- The construction of turn lane and taper improvements in the public right of way along Fall Hill Avenue at Noyack Lane.
- The continued development of Islip Lane. The proffer is to complete the on-site extension of a private street with public access that connects Noble Way to Briscoe Lane. The first leg of the connection is under construction with the Hamptons at Noble site.
- The development of Noyack Lane, which is a new proposed two lane private street with public access serving the project.

As proposed, the site layout also provides for future connections that can be made to the southwest onto the adjacent Graves site to help further the City's goal of creating an integrated grid network between Fall Hill Avenue, Gordon Shelton Boulevard, Cowan Boulevard, and Carl D. Silver Parkway.

The road type labeling on the GDP needs to be revised to match the proffer statement. According to the proffer statement, Noyack and Islip Lanes are proposed to be private streets with public access which is the City's preference. Also, the parking lot lanes within the apartment complex do not meet street standards and should not be labeled as streets.

4. Public Facilities:

Fredericksburg City Schools Operation staff estimates that apartments will generate approximately 0.217 school age children per multi-family unit and 0.458 school age children per townhome unit.

Based on this ratio, this project will generate approximately 62 school age children. By right, the 20.84 acres of R2 zoned property could produce 14 school age children (42 single family homes at 0.32 students per unit). The Applicant has proffered \$900,010 (\$4,545.50 per unit) to offset the capital impacts of these additional students on the school system.

The proposed project is in the service boundary for proposed Fire Station #3. Fire Station #3 is projected to cost \$7 million and is in the five-year Capital Improvement Program, but was not included in the Fiscal Year 2016 or 2017 Capital Budget. The Applicant has proffered \$99,990 (\$505 per unit) to offset their impacts on fire and rescue services.

The Applicant has proffered \$4,000 to the City for two wayside panels for purposes of identifying a historic civil war battlefield to be located within the Fall Hill Avenue right of way to be maintained by the City.

5. Proposed Environmental Changes:

The site is within the Chesapeake Bay Preservation Area. The Applicant has undertaken a perennial streams analysis and determined that the on-site streams are intermittent. As such, the streams are not protected features under the Chesapeake Bay Preservation Act.

The 0.11 acre farm pond is proposed to be moved into a larger proposed storm water facility between the proposed multi-family buildings and Interstate 95 that will treat the stormwater from the development. The pond is conceptually shown over a portion of the intermittent stream.

The Applicant proposes to develop non-tidal wetlands in the middle of the site, which will require coordination with the U.S. Army Corps of Engineers during the land disturbance portion of this proposed project. During that process the Corps of Engineers will determine what if any mitigation is required to offset the impact of developing those wetlands. This may be done through the purchase of off-site offsets. From a local compliance standpoint, the wetlands are not contiguous and connected to a resource (like a perennial stream or the River) and are not a protected feature under the Chesapeake Bay Preservation Ordinance.

6. Architectural Proffers:

The Applicant proposes architectural proffers for the project's buildings. The proffers reference architectural elevations depicting the general character and materials to be used on the residential buildings. The materials include asphalt shingles, PVC trim, vinyl shake siding, vinyl lap siding, and brick. The townhomes will be no less than 18 feet in width. The proffer statement states that, "the general architectural features and materials for the Commercial Project shall be generally similar to the adjoining Mercedes and Volvo commercial car dealerships."

COMPLIANCE WITH THE COMPREHENSIVE PLAN

1. Land Use:

The Future Land Use Map (FLUM) in the 2015 Comprehensive Plan shows the Property as Planned Development - Commercial (PD-C). The PD-C land use "category is reserved for large scale development near major transportation routes," "encourages a wide range of commercial retail and service uses oriented to serve a regional market," and "encourages employment centers that combine office and professional business development within a landscaped, high quality setting"

(pg. 116). The project is within Sub Planning Area 1E, which in addition to the designated land use states that, “planned traffic signals on Fall Hill Avenue fall outside this property’s frontage so access is limited to right-in, right-out. Cross connections across the adjoining property should be considered to gain access to a signalized intersection” (pg. 122).

The “opportunities” listed under Land Use Planning Area 1 (pg. 118) include:

- The redevelopment of Central Park as a regional commercial activity center, through higher levels of mixed use development, and an improved roadway network and bicycle / pedestrian access.
- Construct Fire Station #3 in Celebrate Virginia.
- Protect residential neighborhoods from existing and proposed commercial development through transitional uses and design standards that minimize adverse impacts.
- Enhance this gateway to the City, which is highly visible to travelers in the Interstate-95 corridor, to provide a distinctive and appealing sense of arrival.

The “roads” section of Land Use Planning Area 1 states that “if redevelopment at higher densities is to be feasible, this internal network will need to be reconfigured for greater efficiency. As an example, a cross connection from Carl D. Silver Boulevard to Sub Planning Areas 1D and 1E would provide a transportation link consistent with good urban design” (pg. 123). The “land use potential” discussion relates the Fall Hill Avenue widening project to “intense commercial development” (pg. 121).

Both the Mercedes and Volvo dealerships and Celebrate Virginia have berms and landscaping buffering the development from Fall Hill Avenue. This proposal includes a continuation of the berm and landscaping in front of the Noble car dealerships along the proposed car dealership adjacent to Fall Hill Avenue.

Currently, the Plan envisions an expansion of Central Park and Celebrate Virginia onto the 20 acres proposed here to be rezoned. Is it acceptable for the City to change the Comprehensive Plan vision for these properties?

A total of 884.15 acres of land are designated for current or future PD-C development, in the Future Land Use Map. Currently, residential uses (the Seasons / Havens and Hamptons at Noble) occupy 41.27 acres (5%) of this area. Adding the 16.53 acres of residential use proposed with this project would bring the total to 57.80 acres or 6.5% of the total land area designated as PD-C in the Future Land Use Map.

Currently, 625 dwelling units are located on the 41.27 acres of residential development within the areas designated PD-C Area in the FLUM. Of these, 617 are multi-family units at the Seasons (Havens) and 8 are single family homes on parcels zoned R2. Adding the proposed 78 townhomes and 120 multi-family units would bring the total to 823 units overall (8 single family detached, 78 townhomes, and 737 multi-family units. There are 3,000,000 square feet of non-residential space in the PD-C Area of the FLUM.

The City has engaged Streetsense, an urban planning consultant, to complete Small Area Plans for Area 3 (Route 3 and adjacent neighborhoods) and Area 6 (the area of the City north of the Canal). The Area Plans included a market analysis for Land Use Planning Areas 3 and 6. The market analysis show an over supply of general retail in the two Areas and state that the future of retail in Areas 3 and 6 are more compact mixed use environments, which is in part due to the abundance of

retail in the Central Park and Celebrate Virginia area. The analysis suggests that the City's existing retail shopping centers (the centers along Routes 1 and 3 in particular) should be redeveloped as concentrated mixed-use centers with a more concentrated, denser character.

Streetsense staff was asked if the concepts identified in Areas 3 and 6 could be applied to Area 1. They stated that they are not able to comment on market issues for Area 1 without completing an independent study. However, they did state that due to the characteristics of the site including, its depth, the limitations on creating an east-west through-street across I-95, and the existing presence of adjacent residential units have created a condition where a 300 foot deep commercial frontage along Fall Hill Avenue transitioning to residential use is consistent with their continuing market, land use and planning studies for the City of Fredericksburg. A brief memo they prepared is attached to this report.

2. *Promoting and Sustaining a Liveable Community:*

a. Transportation:

The Transportation chapter emphasizes a coordinated hierarchy of streets and a multi-modal transportation network including networks of sidewalks, trails, and bicycle facilities (pg. 27). The Comprehensive Plan emphasizes "complete streets" (pg. 31). The Plan also establishes a vision for FRED service, VRE services, and other commuting services like GWRideConnect! (pg. 36-37). The Comprehensive Plan emphasizes a connection between transportation and land use and calls for certain principles to be incorporated into new growth including:

- Pedestrian friendly road design.
- Interconnection of new streets with existing streets.
- Connectivity of road and pedestrian networks.
- Preservation of natural areas.
- Mixed use neighborhoods, to include both commercial and residential uses as well as affordable housing and a mix of housing types.
- Reduction of front and side setbacks.
- Reduction of subdivision street widths and turning radii at intersections (pg. 40).

Conformance to this vision may be measured by compliance with the Transportation chapter's Goals (pg. 41). The proposal is consistent with Goals 1. Surface Transportation, 3. Reduce Congestion, 4. Walkability, 5. Complete Streets, and 7. Transportation Safety. The plan would provide a network of complete streets that is scaled appropriately to be safe and functional for pedestrians, bicyclists, drivers, and riders. The plan provides connections to existing development and future development, including to Celebrate Virginia and Central Park. The plan extends a publically accessible road and sidewalk network along Islip Lane, Shadmoor Drive, and out to Fall Hill Avenue along Noyack and Briscoe Lanes. The plan includes the construction of a FRED stop.

b. Public services, public facilities, and preserved open space:

The City of Fredericksburg provides many key public services including, a uniquely unified school system where all City students in a single grade attend the same schools throughout their academic years (pg. 47). The City participates in the Central Rappahannock Regional Library system (pg. 48) and provides water and sewer as well as solid waste and recycling services (pg. 49). The City has its own public safety services made up of Fire and Rescue, the Fredericksburg Police Department, and a General District and Circuit Court (pg. 49-50). The City is planning the development of a

new Fire Station (Fire Station 3) to serve developing areas of the City including the project site. The City also maintains a network of recreational parks and open space that protect valuable natural resources and provide recreational amenities and quality of life for its residents (pg. 51-55).

A project's affect on these facilities may be measured by compliance with the Public services, public facilities, and preserved open space chapter's Goals (pg. 57). The project is consistent with Goals 1. Efficient and Effective Public Services, 2. Safe and Secure Environment, 3. Educational Facilities, 4. Educational System, and 5. Parks and Open Space. The plan provides an inter-connected development pattern which will make the provision of any services dependent on motor vehicles (i.e. school busses, fire trucks, trash trucks, etc.) more efficient. The proposal contains a \$1,000,000 cash proffer to offset impacts on the Fredericksburg school system and the City's Fire and Rescue services. The proposal provides 1.26 acres of parks for the residents of the development and has aligned those improved recreational areas with open spaces on the adjacent Hamptons at Noble site.

Here is a summary of the entitlements approved by the City since the 2014 Moseley study that are projected to increase school attendance:

- a) In 2014, Governors Row was rezoned to R-8 to permit 36 townhome units. The previous zoning of the project permitted the development of 5 single family homes and an office building. In terms of students, the rezoning added 15 additional school students above what was permitted on the site prior to projected enrollment. The project proffers included a \$2,500 per unit (\$90,000) payment to the Community Foundation of the Rappahannock River Region for the benefit of City public schools. The first three townhomes in that project are now under construction.
 - b) In 2015, the Hamptons at Noble was rezoned to CH to permit 128 multi-family units. The previous zoning of the project permitted a vehicle sales establishment and up to 92 multi-family units. In terms of students, the rezoning added 8 additional school students above what was permitted on the site prior to projected enrollment. The project proffers included a total cash payment of \$100,000 to the City to offset capital costs on the schools. The project is currently under construction.
 - c) In 2016, the Mill District was rezoned to PD-MU to permit a mixed use project with 90 multi-family units and 83,500 square feet of non-residential space. The previous zoning of the project permitted 37 residential units and 90,000 square feet of commercial space. In terms of students, the rezoning added 12 additional school students above what was permitted on the site prior to projected enrollment. The project proffers included a \$70,000 lumps sum payment to the City to offset capital costs on the schools. No building permits have been filed for the project.
 - d) In 2016, the Liberty Place project received a special use permit to permit 10 additional multi-family units on their 1.42 acre site. In terms of students, the rezoning added 3 additional school students above what was permitted on the site prior to projected enrollment. Construction permits for the project have been submitted to the City.
- c. Environmental protection:

The environmental protection chapter lays out the City's vision regarding its soils, flood boundaries, streams, storm sewer management programs, woodlands and tree preservation, Rappahannock River, and Wetlands (pg. 62-69). The chapter also discusses the role of Chesapeake Bay Preservation Areas in the City including Resource Protection Areas and Resource Management Areas (pg. 70-71). The environmental chapter also details the City's Green Infrastructure and how it relates to the Regional Green Infrastructure Plan and the Climate, Environment & Readiness Plan (pg. 72-73).

Conformance to this vision may be measured by compliance with the Environmental Protection chapter's Goals (pg. 75). This project is in conformance with Goal 4. Water Quality, 5. Sustainability, and 6. Livability. The proposal will be required to implement best management practices for stormwater management, erosion and sediment control, and wastewater treatment that meet or exceed the Commonwealth of Virginia's regulatory requirements. The project also enhances City sustainability due to the location of the proposed development, which is adjacent to several large business districts and the interconnectivity of its proposed multi-modal transportation network.

The proposal is inconsistent with Goal 3. Natural Functions of Rivers and Streams. The development plan proposes to put stormwater management facilities on top of the existing intermittent stream on-site and to redevelop non-tidal wetlands.

The quality of these features (the stream's intermittent character, its short run, and its termination at a culvert under Interstate 95 as well as the wetland's agricultural nature) should be balanced against the opportunity to locate high density development adjacent to the City's second commercial core in Central Park and Celebrate Virginia, making it more mixed use in nature.

d. Business Opportunities:

The Business Opportunities chapter lays out the City's vision for its Corridors including Fall Hill Avenue (pg. 83) as well as its Business Opportunity Districts (pg. 85). The Plan states that west of I-95 "there are additional large parcels across Fall Hill Avenue from Celebrate Virginia that will also be intensely developed" (pg. 83). The Plan also states that to keep the Central Park and Celebrate Virginia campus economically viable, "the City encourages the progression of these areas to the next level of intensity that includes a more urban mix of uses and promotes high-quality development and redevelopment that is sustainable and attractive within these business opportunity districts" (pg. 85).

Conformance to this vision may be measured by compliance with the Business Opportunity chapter's Goals (pg. 86). The proposal is consistent with Business Opportunity Goals 4. Community Character, 5. Mixed-Uses in Corridors, 6. Complementary and Connected Business Districts. The proposal has a strong urban design centered on complete streets (that are planned to continue into Central Park). The proposal contains commercial and residential uses as appropriate and puts needed residential density next door and interconnected with the City's major retail and office developments.

Goal 3. Business Development states that the City should accommodate and capture its projected share of regional economic growth by providing for retail and office space development in areas

identified for growth. The proposal contains an extension of the existing vehicle sales uses along Fall Hill Avenue. However, three quarters of the proposal is residential use.

e. Residential Neighborhoods and Housing:

The Residential Neighborhoods and Housing chapter encourages “neighborhood design” based on a pedestrian-friendly environment (pg. 90). The chapter addresses existing housing inventory, housing conditions, homeownership, the impact of the University of Mary Washington on housing stocks, and affordable housing (pg. 92-93).

A project’s affect on City housing may be measured by compliance with the Residential Neighborhoods and Housing chapter’s Goals (pg. 95). The proposal is consistent with Residential Neighborhoods Goals 1. Neighborhood Character, 2. Neighborhood Quality, 4. Adequate Public Services and Facilities, 5. Enhanced Connections, 6. Compatible Design and Functionality, 8. Variety of Housing, and 9. Homeownership. The design of the project is based on a quality “neighborhood design” that will be attractive to residents and will make the provision of public services efficient. The design is accessible and multi-modal. The proposal contains both townhouses and multi-family dwellings.

Goal 7. Affordable Housing states that all persons should have the opportunity to rent or purchase safe, decent, and accessible housing within their means. The applicant has proffered that 12 units will meet the affordability criteria of the VHDA’s LIHTC program at 50% AMI, which helps ensure the units affordability to people working in Fredericksburg’s economy. The remainder of the proposed units should be considered market rate.

COMPLIANCE WITH THE UNIFIED DEVELOPMENT ORDINANCE

1. Existing zoning:

The Property is currently zoned R2 and is surrounded by CH zoning to the east, R2 zoning to the south and west, and Planned Development-Commercial (PD-C) zoning to the north. The Property was part of the acreage annexed to the City in 1984. At that time it was assigned an R1 zoning designation, which was a holding zone until a property was deemed ready for development. The property was part of the City initiated rezoning in 2014 that rezoned all remaining R1 zoned land to the R2 zoning district.

§ 72-31.2.A states that the purpose of the R2 zoning district is “to provide for single-family detached dwellings in suburban-style subdivisions at a density not to exceed two dwelling units per acre. The district also allows selected uses which are compatible with the low density residential character of the district, and to implement the stated purposes and intent of the Comprehensive Plan.”

R2 permits two single family homes per acre on minimum 15,000 square foot lots. The 20.84 acres could yield 42 single family homes by-right. The Future Land Use Map of the City’s Comprehensive Plan calls for the area to be Planned Development-Commercial (pg 121). The

Comprehensive Plan also calls for Mixed Use in Corridors and Complementary and Connected Business Districts (pg 86). The City is also upgrading Fall Hill Avenue to four lanes with a curb median. This vision contemplates a level of intense development that exceeds, in use and density, what may be built in the R2 zoning district.

2. *Proposed zoning:*

a. Commercial Highway.

§ 72-32.4.A states that the purpose of the CH zoning district is “to provide locations on heavily traveled collector and arterial highways for those commercial and service uses which are oriented to the automobile and require good access but not dependence on adjacent uses or pedestrian trade.” The purpose also states that “the district shall generally be considered inappropriate in those parts of the City where individual uses can be grouped in preplanned concentrations and in newly developing areas, such as those in areas that have been annexed to the City.”

The Property is within a newly developing area of the City that was annexed in 1984. The area proposed to be zoned CH is 4.31 acres adjacent to CH zoned land. The 4.31 acres is proposed to be a Vehicle Sales establishment and would be part of a concentration of Vehicle Sales establishments already developed along Fall Hill Avenue. Only two zoning districts permit vehicle sales by-right, the CH zoning district and the Planned Development-Commercial zoning district.

b. R-12, Residential.

§72-31.5.A states that the purpose of the R12 District is to “provide for a planned mixture of single-family attached and multifamily dwelling types at a density not to exceed 12 dwelling units per acre. Development in this district shall be sensitive to existing land physiography, adequate public facilities and infrastructure, transportation access requirements and vulnerable environmental features in achieving optimal siting of dwellings, open space, recreational and community facilities, and transportation systems.”

The proposal is a mixture of single family attached and multifamily dwelling types at 11.96 units per acre. There are adequate public facilities and infrastructure serving the site. The proposal includes optimal open space, recreational and community facilities. The transportation infrastructure proposed with the project is a major positive. The storm water management area of the project, however, is proposed to encroach on an intermittent stream.

c. Comprehensive Plan proposed zoning.

The Comprehensive Plan’s FLUM envisions this parcel of land being rezoned to Planned Development-Commercial.

§ 72-33.2.A, purpose of the Planned Development – Commercial District states in part, “the district should be reserved for development on contiguous land areas of at least 150 acres under single ownership or control capable of containing an aggregate gross floor area in excess of 500,000 square feet.” This site is only 20.84 acres.

§ 72-33.2.D.4 limits residential uses to ten percent of the overall PD-C district and 24 units per acre. As stated above, overall residential use in the PD-C designated area of the FLUM would be 57.80 acres or 6.5% of the total 884.15 acres. Adding the proposed 78 townhomes and 120 multi-

family units would bring the total to 823 units overall (8 single family detached, 78 townhomes, and 737 multi-family units. The total units per acre in the FLUM PD-C area would be 0.93 units per acre if this project were approved. There are 3,000,000 square feet of non-residential space in the PD-C Area of the FLUM.

Townhomes are a special use in the PD-C zoning district and multi-family units are permitted by-right.

CONCLUSION AND RECOMMENDATION

The City Council should approve this rezoning. The Property is proposed to be intensely developed and is well suited to handle a high density. The Property is also in an area where the City is investing in infrastructure to support more use. The Property is currently a cornfield and is located in the middle of arguably the City's most intense growth area. This is a conclusion drawn while balancing the location and character of the site against the impact the proposed development would have on the sites two minor environmental features.

The proposed zoning districts do not match the color on the Future Land Use Map. This area of the City is designated for commercial and office development. However, the urban pattern proposed is consistent with the City's vision of a more intense mixed-use core developing in this area. Similarly, if the 884 acre area shown on the Future Land Use Map was all one planned unit development zoned PD-C, this type of residential use may be the most appropriate on this particular property. The property's frontage on Fall Hill Avenue is proposed to be part of a larger vehicle sales cluster. The property has relatively limited access from Fall Hill Avenue and is not at a signalized intersection where more intense commercial activity would be appropriate. The proposed use and layout conform to adjacent land uses to the north, east, and west and the area. The R2 zoned land to the south is planned to be even more intense development. The use types, development pattern, urban design and the proposed mitigation to the impacts the development would have on public facilities also meets the intent of the Comprehensive Plan.

The existing zoning R2 zoning on the property is not sufficient to meet the intent of the Comprehensive Plan or the level of infrastructure investment the City has committed to along the Fall Hill Avenue corridor west of Interstate 95. In order to prevent the proliferation of strip retail development, CH zoning is not envisioned to be expanded into the developing areas of the City. However, this proposal is to expand an existing CH zoned area to provide for an expansion of an existing vehicle sales cluster. The residential portions of the property meet the intent of the R12 zoning district in that they are well designed, provide multi-modal transportation opportunities, and ample open and recreational space for residents. The residential land use, with the addition of the final proffer, includes an element that will help achieve the City's goals for affordable housing. Finally, the PD-C zoning district is meant for sites of 150 contiguous acres.

The Applicant's proposal is to use two zoning districts to accomplish a complimentary, well-connected and designed development program that fits into its surroundings, context, and City planning and vision.

ATTACHMENTS

1. Proposed ordinance approving rezoning and resolution denying rezoning.
2. Proffer statement and exhibits.
3. Letters of support.

4. Letter of Commitment from the Applicant.



November 22, 2016
Regular Meeting
Ordinance No. 16-__

MOTION:

SECOND:

RE: REZONING 20.84 ACRES OF LAND LOCATED ON THE SOUTH SIDE OF FALL HILL AVENUE, BETWEEN BRISCOE LANE AND I-95, FROM RESIDENTIAL R-2 TO COMMERCIAL HIGHWAY AND R-12, WITH CONDITIONS

ACTION: APPROVED; Ayes: 0; Nays: 0

First read: _____ **Second read:** _____

IT IS HEREBY ORDAINED by the Fredericksburg City Council that the official zoning map of the City, established pursuant to City Code §72-30, is amended as follows:

I. Background Information

This is a request to rezone 20.84 acres of land from Residential R-2 to Commercial Highway and R-12 with proffered conditions. The property is identified on the City's Geographic Information System as GPIN 7769-77-8378 and a portion of GPIN 7769-87-3295. The property is owned by Carol B. Coleman, Bonnie B. Carter, and Gloria B. Whittaker. The applicant is Hamptons at Family, L.P., a Virginia limited partnership, the contract purchaser. The property is located on the south side of Fall Hill Avenue, west of I-95 and east of Briscoe Lane. The applicant proposes to develop 4.31 acres as Commercial Highway, and the remaining 16.53 acres as single family attached and multi-family residential.

In connection with the application, the applicant has submitted a General Development Plan entitled "The Hamptons Phase II," by Bowman Consulting, dated May 2016, last revised 6/20/16, comprising sheets 1 through 8; and a Voluntary Proffer Statement dated November 2, 2016.

In adopting this ordinance, City Council has considered the applicable factors in Virginia Code § 15.2-2284. The City Council has determined that public necessity, convenience, general welfare and good zoning practice favor the requested rezoning.

II. Official Zoning Map Amendment

The official zoning map, prepared in accordance with City Code §72-30, is hereby amended by rezoning the following described land, as shown more particularly on a plat of survey entitled "Rezoning Plat, The Hamptons Phase II," by Kevin D. Early, Land Surveyor, included in the General Development Plan as sheet 3 of 8:

- A. GPIN 7769-77-8378, consisting of +/- 0.987 acres of land, is rezoned from Residential R-2 to Residential R-12.

- B. A +/- 19.853 acre portion of GPIN 7769-87-3295 is rezoned from Residential R-2 to Commercial Highway CH and Residential R-12.

The new zoning district lines are shown on sheet 4 of the General Development Plan.

III. Proffered Conditions

This is a conditional rezoning. The voluntary proffers contained in the Voluntary Proffer Statement are accepted and shall govern the use and development of this land.

IV. Effective Date

This ordinance is effective immediately. The applicant shall record a certified copy of this ordinance with a notice of conditional zoning, in a form approved by the City Attorney, in the land records of the Fredericksburg Circuit Court Clerk, with the owner as the "grantor" and the City as the "grantee," within 30 days of the adoption of this ordinance.

Votes:

Ayes:

Nays:

Absent from Vote:

Absent from Meeting:

Approved as to form:

Kathleen Dooley, City Attorney

Clerk's Certificate

I, the undersigned, certify that I am Clerk of Council of the City of Fredericksburg, Virginia, and that the foregoing is a true copy of Ordinance No. 16- duly adopted at a meeting of the City Council meeting held November 22, 2016 at which a quorum was present and voted.

Tonya B. Lacey, CMC
Clerk of Council



MOTION:

November 22, 2016

SECOND:

Regular Meeting

Resolution 16-__

RE: Denying the Hamptons at Family, L.P. rezoning application

ACTION: APPROVED: Ayes:0; Nays: 0

This is a request to rezone 20.84 acres of land from Residential R-2 to Commercial Highway and R-12 with proffered conditions. The property is identified on the City's Geographic Information System as GPIN 7769-77-8378 and a portion of GPIN 7769-87-3295. The property is owned by Carol B. Coleman, Bonnie B. Carter, and Gloria B. Whittaker. The applicant is Hamptons at Family, L.P., a Virginia limited partnership, the contract purchaser. The property is located on the south side of Fall Hill Avenue, west of I-95 and east of Briscoe Lane. The applicant proposes to develop 4.31 acres as Commercial Highway, and the remaining 16.53 acres as single family attached and multi-family residential.

In connection with the application, the applicant has submitted a General Development Plan entitled "The Hamptons Phase II," by Bowman Consulting, dated May 2016, last revised 6/20/16, comprising sheets 1 through 8; and a Voluntary Proffer Statement dated November 2, 2016.

In adopting this resolution, City Council has considered the applicable factors in Virginia Code § 15.2-2284. The City Council has determined that public necessity, convenience, general welfare and good zoning practice do not favor the requested rezoning.

The City Council hereby resolves that:

- The application is not consistent with the 2015 Fredericksburg Comprehensive Plan, which shows the Property as Planned Development - Commercial (PD-C). The Property is identified in the future land use map as Land Area 1E, which is expected "to experience intense commercial development."
- The application would increase the number of acres for single family attached or multifamily residential development from the planned 2 acres to a proposed 16.53 acres; the project would result in an increase of the number of residential units from a planned maximum of 50 to a proposed maximum of 198.
- The application does not include a proffer for the phasing of the commercial and residential aspects of the development.
- The applicant proposes a floor area ratio of 0.70 for the automobile sales use, but this FAR is not consistent with neighboring automobile sales uses.

- The proposed site planning for the multifamily buildings produces a “garden apartments” design which is not consistent with the 2015 Comprehensive Plan goals for neighborhood character, neighborhood quality, and distinct and attractive neighborhoods.

The City Council therefore denies approval of the application.

Votes:

Ayes:

Nays:

Absent from Vote:

Absent from Meeting:

Clerk's Certificate

I certify that I am Clerk of Council of the City of Fredericksburg, Virginia, and that the foregoing is a true copy of Resolution No. 16-__, adopted at a meeting of the City Council held November 22, 2016, at which a quorum was present and voted.

Tonya B. Lacey, CMC

Clerk of Council

VOLUNTARY PROFFER STATEMENT

Project Name: The Hamptons Phase II

Applicant &

Contract Purchaser: Hamptons at Family, L.P., a Virginia limited partnership
440 Monticello Avenue, Suite 1700
Norfolk, VA 23510
Attn: Richard Counselman

Counsel: Charles W. Payne, Jr. Esq.
Hirschler Fleischer
725 Jackson Street; STE 200
Fredericksburg, VA 22401

Owner: Carol B. Coleman, Bonnie B. Carter & Gloria B. Whittaker

Property: GPIN# 7769-87-3295 (portion of) & GPIN# 7769-77-8378, known as 44 Briscoe Lane, City of Fredericksburg, Virginia (“City”), and consisting of approximately 20.840 acres, located between Fall Hill Avenue and I-95, all as generally depicted on the GDP (as defined below), all of the aforesaid collectively the “Property”

GDP: That certain generalized development plan prepared by Bowman Consulting and titled “The Hamptons Phase II, General Development Plan”, dated May 2016, as last revised October 19, 2016 (the “GDP”)

Request: Rezoning from R-2 to Commercial Highway (“CH”) and Residential-12 (“R-12”) in accordance with Section 72-22.4, et al., of the City’s Uniform Development Code (“UDO”)

City Case No: RZ2016-03

Date: November 15, 2016

The Owner and Applicant voluntarily proffer the conditions listed below which shall be applied to the Property if it is rezoned to the Commercial Highway (“CH”) and Residential-12 (“R-12”) zoning districts, as shown on the GDP. These conditions are proffered as a part of the requested rezoning and the Owner and Applicant acknowledge that they are voluntary and reasonable in accordance with Sections 15.2-2298 and 15.2-2303, et al. of the Code of Virginia (1950), as amended (collectively the “Proffers”). In the event the above-referenced rezoning is not approved by the City Council of the City of Fredericksburg, Virginia (“City”), then the Proffers shall be withdrawn, and thus null and void. Upon approval by the City of this rezoning and these Proffers, any and all prior proffers encumbering the Property shall be null and void and of no further force and effect, and the Proffers shall supersede said prior proffers, and thereafter be in

full force and effect. The Proffers shall run with and encumber the Property or any portion thereof and are binding upon all future assignees, successors, grantees, or lessees thereof. The headings of the Proffers set forth below have been prepared for convenience or reference only and shall not control or affect the meaning or be taken as an interpretation of any provision of the Proffers.

1. **LAND USE**

A. The Property is to be rezoned under the City’s CH and R-12 zoning districts, for purposes of developing and constructing the following:

(i) under the CH zoning district for a portion of the Property as shown on the GDP, an automotive sales dealership (the “Commercial Project”); and

(ii) under the R-12 zoning district for a portion of the Property as shown on the GDP, no greater than 78 single family attached dwellings (the “SFA Project”); and (ii) five multifamily buildings, containing a total of 120 units (the “Apartment Project”).

(iii) subdivision of the Property shall be in general accordance with the attached GDP.

(iv) all of the above being known collectively as the “Project”.

B. Generalized Development Plan: The Property shall be developed in general conformance with the GDP, which is attached hereto, incorporated herein by this reference and marked as Exhibit A. For purposes of the final site and subdivision plans, minor adjustments may occur for purposes of addressing final site plans, engineering, design requirements and/or compliance with federal or state agency regulations including, but not limited to, Virginia Department of Transportation (“VDOT”), Virginia Department of Environmental Quality (“DEQ”), Army Corps of Engineers, etc., and compliance with the requirements of the City’s development regulations and design standards manual. Notwithstanding the foregoing, any modifications or adjustments to the final plans, as noted above, shall be approved by the City Zoning Administrator.

2. **TRANSPORTATION**

A. **Briscoe Lane Improvements.** The Applicant shall improve portions of Briscoe Lane (including curb and gutter and sidewalk area) and dedicate 0.576 acres of public right of way, all in the locations shown and depicted on the GDP (collectively the “Briscoe Lane Improvements”). The Briscoe Lane Improvements shall be designed and constructed in accordance with standards for public streets under the City Uniform Development Code (“City Code”) §72-52.1, et al. The Applicant shall substantially complete the Briscoe Lane Improvements prior to the issuance of a certificate of occupancy by the City for the first multifamily building of the Apartment Project.

- B. **Shadmoor Drive Improvements.** The Applicant shall dedicate public right of way and construct Shadmoor Drive from Noble Way to Briscoe Lane, all as shown and described on the GDP (“Shadmoor Drive Improvements”). The Shadmoor Drive Improvements shall be designed and constructed in accordance with standards for public streets under City Code §72-52.1, et al. The Applicant shall substantially complete said improvements prior to the issuance of a certificate of occupancy by the City for the first multifamily building of the Apartment Project. Notwithstanding anything to the contrary herein, prior to the connection of Shadmoor Drive to Noble Way, the residents of the Project will have ingress and egress access, whether through public dedication or through public or private easement, across Noble Way to and from Fall Hill Avenue.
- C. **Extension of Sidewalk Network.** The Applicant agrees to extend and connect a portion of the sidewalk along Fall Hill Avenue within the area shown and depicted on the GDP as “Connect to Existing Sidewalk” (the “Extended Sidewalk Area”). The Applicant agrees to substantially complete the Extended Sidewalk Area prior to the issuance of a certificate of occupancy by the City for the first multifamily building of the Apartment Project. Notwithstanding the foregoing, in the event the Applicant is unable (at no fault of the Applicant) to complete the Extended Sidewalk Area prior to the issuance of a certificate of occupancy by the City for the first multifamily building of the Apartment Project, the Applicant shall pay the City a cash proffer of \$16,700.00 (which reflects the costs of the Extended Sidewalk Area) in lieu of constructing the Extended Sidewalk Area.
- D. **FRED Stop.** Subject to the approval of FRED Regional Transit (“FRED”), the Applicant shall locate a FRED stop within the Property, which shall include applicable FRED stop signage and a shelter. If requested by FRED, the Applicant agrees to locate and install the FRED stop as described herein on or before the first certificate of occupancy for the first residential apartment building.
- E. **Crosswalk.** The Applicant agrees to install a crosswalk across Fall Hill Avenue at its intersection with Gordon W. Shelton Boulevard within the area shown and depicted on the GDP as “PROPOSED CROSSWALK” (the “Crosswalk”). The Applicant agrees to substantially complete the Crosswalk prior to the issuance of a certificate of occupancy by the City for the first multifamily building of the Apartment Project. Notwithstanding the foregoing, in the event the Applicant is unable (at no fault of the Applicant) to complete the Crosswalk prior to the issuance of a certificate of occupancy by the City for the first multifamily building of the Apartment Project, then the Applicant shall pay the City a cash proffer of \$4,100.00 (which reflects the costs of the Crosswalk) in lieu of installing the Crosswalk.
- F. **Fall Hill Avenue Improvements.** The Applicant shall dedicate certain right of way and construct improvements along Fall Hill Avenue to include a 150' right turn lane and a 150' taper off of Fall Hill Avenue and onto Noyack Lane, all within the areas shown and depicted on the GDP (“Fall Hill Avenue Access Improvements”). The

Fall Hill Avenue Access Improvements shall be designed and constructed in accordance with standards for public streets under City Code §72-52.1, et al. The Applicant shall substantially complete said improvements on or before the City issuance for the certificate of occupancy for the first residential apartment building within the Apartment Project.

G. **Public Access.** Upon the City’s request and after the Applicant has completed the transportation improvements described herein, the Applicant agrees to grant public ingress/egress and pedestrian access across the following private roads within the Project (all as shown on the GDP):

(i) across Noyack Lane from its intersection with Fall Hill Avenue to its intersection with Islip Lane; and

(ii) across Islip Lane from its intersection with Briscoe Lane to its interparcel connection with The Hamptons Phase I.

3. **ARCHITECTURAL FEATURES**

A. **General Features for Apartment Project.** The general architectural features and design for the Apartment Project shall be as depicted on the attached Exhibit B, which exhibit is incorporated herein by this reference (the “Apartment Project Elevations”).

B. **Materials for Apartment Project.** The building materials for the Apartment Project shall be as provided on Exhibit B.

C. **General Features & Materials for SFA Project.** The units for the SFA Project shall be no less than 18 feet in width, and include one or two car garages, and the building materials shall be substantially similar to the Apartment Project.

D. **General Features & Materials for Commercial Project.** The general architectural features and design and materials for the Commercial Project shall be generally similar to the adjoining Mercedes and Volvo commercial car dealerships.

4. **CASH PROFFERS.** For purposes of mitigating potential impacts to City school capital facilities and fire and rescue capital facilities, the Applicant, in accordance with the City’s underlying UDO and applicable state code stated herein, shall pay an aggregate total of up to One Million Dollars (\$1,000,000.00) in cash proffers for the Apartment Project and SFA Project pursuant to the following payment schedule:

A. for purposes of the Apartment Project, the Applicant shall pay \$5,050.50 per unit, which shall be payable prior to the issuance of a certificate of occupancy for each multifamily building within the Apartment Project; and

B. for purpose of the SFA Project, the Applicant shall pay \$5,050.50 per unit, which shall be payable prior to the issuance of a certificate of occupancy for each unit within the SFA Project.

C. for purposes of the foregoing proffers Section 4 A. and 4 B., the per unit cash proffer shall be applied as follows:

(i) \$505.00 per unit (aggregate of \$99,990.00) for fire and rescue public facilities; and

(ii) \$4,545.50 per unit (aggregate of \$900,010.00) for public school facilities.

5. **AMENITIES.** The Applicant will construct and locate on the Property a club house and pool, all as generally depicted on the GDP, which will be completed prior to the issuance of a certificate of occupancy for the first residential apartment building.
6. **WAYSIDE PANEL CASH PROFFER.** The Applicant agrees to pay \$4,000 to the City as a cash proffer for the cost to acquire two wayside panels for purposes of identifying a historic civil war battlefield. The Applicant agrees to pay said proffer prior to the City issuance of a certificate of occupancy for the first residential apartment building of the Project. The wayside panels shall be located within the Fall Hill Avenue right of way and shall be maintained by the City.
7. **LANDSCAPING PROFFER.** For purposes of the commercial automotive sales facility located along Fall Hill Avenue, the Applicant agrees to extend west the landscape buffer provided along Fall Hill Avenue (fronting the current Mercedes and Volvo dealerships) and to include the following features:
 - A. 20' wide landscape buffer from the edge of the dedicated right of way for Fall Hill Avenue and extending south toward the dealership all in the area shown on the GDP;
 - B. along with a berm that includes a minimum height of 2.5' within the said buffer. The berm shall not be aligned in a straight line, but rather shall meander within the said buffer area in a manner consistent with the adjoining buffer to the east (e.g. Volvo dealership);
 - C. tree planting within said buffer to include trees with a diameter of least 3.5 inches at breast height on 30' centers, except the said trees shall not be located closer than seventy (70) feet to any access area along Fall Hill Avenue;
 - D. the Applicant agrees to trim the trees so a minimum of seven (7) feet of base of each tree measured from the base of the berm will be free of branches; and
 - E. the Applicant will also plant four ornamental shrubs selected by the Applicant between each 30' tree section, and the ornamental shrubs shall not exceed the height of the said berm.

F. All of the foregoing landscaping proffers under this Section 7 shall be completed prior to the issuance of a certificate of occupancy for the proffered automotive sales facility.

8. **RESERVATION OF UNITS.** Notwithstanding anything to the contrary under the Proffers and for purposes of the Apartment Project, the Applicant agrees to reserve up to ten percent (10%) of the total number of Apartment Project units for qualified residents with household incomes at fifty percent (50%) of the Washington DC metropolitan statistical area (“Reserved Units”). The rent for the Reserved Units will be limited to a 30% rent burden (e.g. 30% of 100% of said residents' respective incomes less the utility allowance). The Reserved Units shall be applicable to one and two bedroom units only and for a period not to exceed five (5) years after the issuance of a certificate of occupancy for the first multifamily building.

[AUTHORIZED SIGNATURES TO FOLLOW]

WITNESS the following signatures acknowledging approval and consent of this proffer statement:

APPLICANT/CONTRACT PURCHASER:

Hamptons at Family, L.P.,
a Virginia limited partnership

By: Hamptons at Family GP, L.L.C., its General Partner

By: _____

Print Name: _____

Title: _____

By: _____

Print Name: _____

Title: _____

COMMONWEALTH OF VIRGINIA,
CITY/COUNTY/TOWN OF _____, to wit:

I, the undersigned, a Notary Public in and for the jurisdiction aforesaid, do hereby certify that _____, in his/her capacity as _____ for Hamptons at Family GP, L.L.C, the General Partner of Hamptons at Family, L.P. has personally acknowledged the same before me in my aforesaid jurisdiction.

GIVEN under my hand and seal this _____ day of November, 2016.

Notary Public

Print Name: _____

My Commission Expires: _____

Registration No.: _____ [SEAL]

COMMONWEALTH OF VIRGINIA,
CITY/COUNTY/TOWN OF _____, to wit:

I, the undersigned, a Notary Public in and for the jurisdiction aforesaid, do hereby certify that _____, in his/her capacity as _____ for Hamptons at Family GP, L.L.C, the General Partner of Hamptons at Family, L.P. has personally acknowledged the same before me in my aforesaid jurisdiction.

GIVEN under my hand and seal this _____ day of November, 2016.

Notary Public

Print Name: _____

My Commission Expires: _____

Registration No.: _____ [SEAL]

OWNERS

By: _____
Carol B. Coleman

COMMONWEALTH OF VIRGINIA,
CITY OF FREDERICKSBURG, to wit:

I, the undersigned, a Notary Public in and for the jurisdiction aforesaid, do hereby certify that Carol B. Coleman has personally acknowledged the same before me in my aforesaid jurisdiction.

GIVEN under my hand and seal this _____ day of November, 2016.

Notary Public

Print Name: _____
My Commission Expires: _____
Registration No.: _____
[SEAL]

By: _____
Bonnie B. Carter

COMMONWEALTH OF VIRGINIA,
CITY OF FREDERICKSBURG, to wit:

I, the undersigned, a Notary Public in and for the jurisdiction aforesaid, do hereby certify that Bonnie B. Carter has personally acknowledged the same before me in my aforesaid jurisdiction.

GIVEN under my hand and seal this _____ day of November, 2016.

Notary Public

Print Name: _____
My Commission Expires: _____
Registration No.: _____
[SEAL]

By: _____
Gloria B. Whittaker

COMMONWEALTH OF VIRGINIA,
CITY OF FREDERICKSBURG, to wit:

I, the undersigned, a Notary Public in and for the jurisdiction aforesaid, do hereby certify that Gloria B. Whittaker has personally acknowledged the same before me in my aforesaid jurisdiction.

GIVEN under my hand and seal this _____ day of November, 2016.

Notary Public

Print Name: _____
My Commission Expires: _____
Registration No.: _____
[SEAL]

EXHIBIT A

GDP

GENERAL DEVELOPMENT PLAN

THE HAMPTONS PHASE II

WARD 1

CITY OF FREDERICKSBURG, VIRGINIA

SHEET INDEX

NO.	DESCRIPTION
1.	COVER SHEET
2.	LEGEND, ABBREVIATIONS, AND ROAD SECTIONS
3.	REZONING PLAT
4.	SCHEMATIC PLAN
5.	EX. CONDITIONS / ENVIRONMENTAL ANALYSIS PLAN
6.	LANDSCAPE & SIGNAGE PLAN
7.	TRANSPORTATION ANALYSIS & PEDESTRIAN PLAN
8.	PUBLIC FACILITIES ASSESSMENT PLAN

APPLICANT

HAMPTONS AT FAMILY, L.P.
440 MONTICELLO AVENUE, SUITE 1700
NORFOLK, VA 23510
CONTACT: RICHARD COUNSELMAN

PLANNER/ ENGINEER

BOWMAN CONSULTING GROUP
650A NELMS CIRCLE
FREDERICKSBURG, VA 22406
P: (540) 371-0268
F: (540) 371-3479
CONTACT: MR. WILLIAM S. PYLE, P.E.

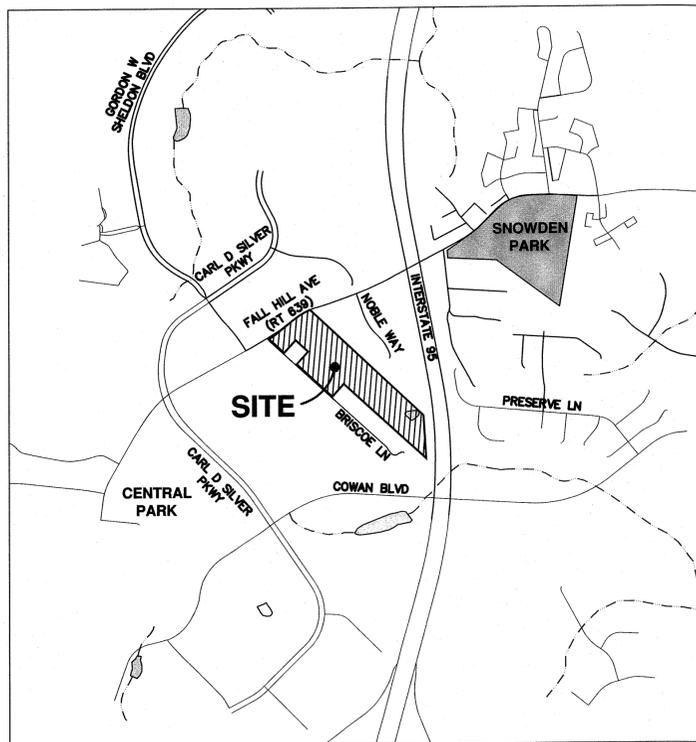
OWNER

CAROL B. COLEMAN, BONNIE B. CARTER &
GLORIA WHITTAKER

LAND USE ATTORNEY

CHARLES W. PAYNE, JR.
C/O HIRSCHLER FLEISCHER
725 JACKSON STREET, SUITE 200
FREDERICKSBURG, VA 22401
PHONE: (540) 604-2108

VICINITY MAP
1" = 1000'



GENERAL NOTES

1. THE AREA TO BE REZONED FOR THIS PROJECT CONSISTS OF A ±20.84 ACRE PORTION OF THE PARCELS IDENTIFIED AS TAX MAP 272-91 (GPIN 7769-87-3295) AND 272-91C (GPIN 7769-77-8378).
2. THE PROPERTIES DELINEATED HEREON CURRENTLY STAND IN THE NAMES OF GLORIA B. WHITTAKER, BONNIE B. CARTER AND CAROL B. COLEMAN AS RECORDED IN DEED BOOK 208 AT PAGE 100 AND DEED BOOK 208 AT PAGE 97 IN THE CLERK'S OFFICE OF THE CITY OF FREDERICKSBURG AND IS SHOWN AS A PORTION OF GPIN: 7769-87-3295 AND GPIN: 7769-77-8378.
3. THE SUBJECT PROPERTY IS CURRENTLY VACANT AND ZONED R2 RESIDENTIAL. THE PROPOSED ZONING FOR THIS SITE IS C-H, COMMERCIAL-HIGHWAY DISTRICT AND R-12 RESIDENTIAL DISTRICT.
4. BOUNDARY AND MERIDIAN SHOWN HEREON, AND REZONING PLAT HEREIN, ARE BASED ON A PLAT ENTITLED "PLAT OF BOUNDARY AND TOPOGRAPHIC SURVEY TAX MAP 272 PARCEL 91" PREPARED BY BAGBY, CALDWELL AND ASSOCIATES, P.C., DATED FEBRUARY 2005 AND DOES NOT REPRESENT A CURRENT FIELD RUN SURVEY BY BOWMAN CONSULTING. ADDITIONAL PROPERTY INFORMATION WAS DERIVED FROM CITY OF FREDERICKSBURG GIS DATA.
5. ADDITIONAL TOPOGRAPHY SHOWN IN THIS PLAN WAS OBTAINED FROM THE CITY OF FREDERICKSBURG GIS. THE CONTOUR INTERVAL IS 2 FEET.
6. THE SITE DOES NOT LIE WITHIN A FLOODPLAIN OVERLAY DISTRICT.
7. THE SITE IS LOCATED WITHIN THE CHESAPEAKE BAY OVERLAY DISTRICT, HUB ZONE TRACT 5, AND THE CELEBRATE VIRGINIA SOUTH TOURISM ZONE.
8. THERE ARE COMMERCIAL AND RESIDENTIAL USES PROPOSED ON THIS SITE.
9. THERE ARE NO KNOWN CEMETERIES OR HISTORIC STRUCTURES LOCATED ON THE SUBJECT PROPERTY. MORE SPECIFICALLY, BASED ON A REVIEW OF THE VIRGINIA DEPARTMENT OF HISTORIC RESOURCES' (DHR) VIRGINIA CULTURAL RESOURCE INFORMATION SYSTEM (VCRIS), THERE ARE NO DOCUMENTED ARCHAEOLOGICAL SITES OR HISTORIC RESOURCES WITHIN THE PROPERTY LIMITS. TWO HOUSES LOCATED IMMEDIATELY ADJACENT TO THE PROPERTY ALONG BRISCOE LANE (DHR IDS 111-5321 AND 111-5278) ARE IDENTIFIED BY DHR, BUT WERE DETERMINED TO BE NOT ELIGIBLE FOR LISTING ON THE NATIONAL REGISTER OF HISTORIC PLACES (NRHP). A THIRD HOUSE (GRAVES HOUSE, 3430 FALL HILL AVENUE, DHR ID 111-5285) IS LOCATED OFFSITE AND JUST TO THE SOUTH OF THE PROPERTY; THIS RESOURCE WAS ALSO DETERMINED TO BE NOT ELIGIBLE FOR LISTING ON THE NRHP BY DHR. THE PROPERTY IS LOCATED WITHIN THE DESIGNATED BANK'S FORD/SALEM CHURCH BATTLEFIELD DISTRICT (DHR ID 088-5181). HOWEVER, GIVEN THE SURROUNDING LAND USES AND CURRENT SITE CONDITIONS, NO ADVERSE IMPACTS TO HISTORIC RESOURCES ARE ANTICIPATED AS A RESULT OF THE PROPOSED DEVELOPMENT ACTIVITIES AT THE PROPERTY.
10. THE PROPERTY IS LOCATED IN ZONE "X" (AREAS DETERMINED TO BE OUTSIDE 500 YEAR FLOODPLAIN) AS SHOWN ON FEMA FLOOD INSURANCE RATE MAPS FOR THE CITY OF FREDERICKSBURG, VIRGINIA, COMMUNITY-PANEL NUMBER 5100650020C; EFFECTIVE DATE SEPTEMBER 19, 2007.
11. THE PROPOSED SITE WILL BE SERVED BY PUBLIC WATER AND SEWER MAINTAINED BY THE CITY OF FREDERICKSBURG.
12. THE PROPOSED SITE WILL CONTAIN PRIVATE ROADS MAINTAINED BY THE PROPERTY OWNERS; AND PUBLIC STREETS TO BE MAINTAINED BY THE CITY OF FREDERICKSBURG.
13. THE STREETS SHALL BE DESIGNED IN ACCORDANCE WITH THE CITY OF FREDERICKSBURG'S DESIGN STANDARDS MANUAL.
14. STORMWATER MANAGEMENT FACILITY(S) AND OPEN SPACE SHALL BE MAINTAINED BY THE PROPERTY OWNER(S).
15. DIMENSIONS, AREAS, BUILDING SQUARE FOOTAGES, AND LAYOUT ARE APPROXIMATE IN NATURE AND SUBJECT TO CHANGE WITH FINAL ENGINEERING.
16. EROSION CONTROL FOR THIS SITE WILL BE PROVIDED THROUGH ONSITE MEASURES IN ACCORDANCE WITH THE VIRGINIA EROSION AND SEDIMENTATION CONTROL HANDBOOK.
17. STORMWATER SHALL BE CONTROLLED THROUGH A STORM DRAINAGE SYSTEM IN ACCORDANCE WITH THE VASWMH.

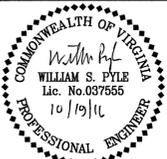
Bowman
CONSULTING

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COVER SHEET
THE HAMPTONS PHASE II
GENERAL DEVELOPMENT PLAN

WARD 1

R2 2016-03
CITY PROJECT NUMBER



PLAN STATUS	
03/31/16	PRE-APP SUBMISSION
06/20/16	1ST SUBMISSION
08/09/16	2ND SUBMISSION
10/11/16	ADDRESS COMMENTS
10/19/16	ADDRESS COMMENTS

DATE	DESCRIPTION
BO/SG	SG WSP
DESIGN	DRAWN CHKO
SCALE	H: AS NOTED V:

JOB No. 9432-01-001

DATE : MAY 2016

FILE No. 9432-D-ZP-001

SHEET 1 OF 8

03/31/16	PRE-APP SUBMISSION
06/20/16	1ST SUBMISSION
08/09/16	2ND SUBMISSION
10/11/16	ADDRESS COMMENTS
10/19/16	ADDRESS COMMENTS

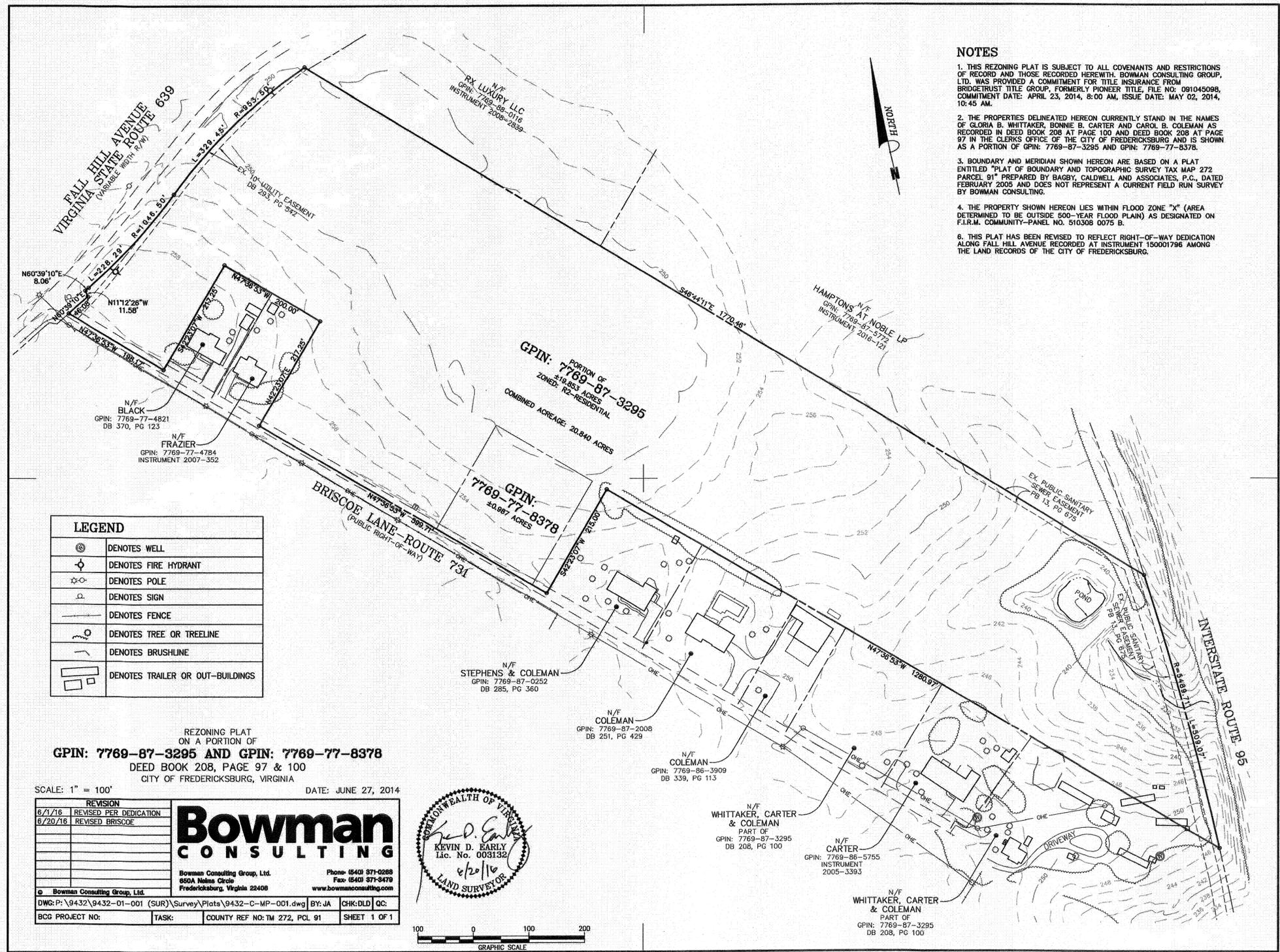
BO/SG	SG	WSP
DESIGN	DRAWN	CHKD

LEGEND

EXISTING	DESCRIPTION	PROPOSED
	INDEX CONTOUR	
	INTERMEDIATE CONTOUR	
	EDGE OF PAVEMENT	
	CURB AND GUTTER	
	TRANSITION FROM CG-6 TO CG-6R	
	PROPOSED HEADER CURB	
	PROPERTY LINE	
	DEPARTING PROPERTY LINE	
	LOT LINE	
	RIGHT-OF-WAY	
	CENTERLINE	
	FLOOD PLAIN	
	CLEARING AND GRADING	
	TREE LINE	
	FLOW LINE OF SWALE	
	STREAM	
	OVERLAND RELIEF PATHWAY	
	FENCE LINE	
	EASEMENT	
	WATER LINE	
	WATER VALVE	
	REDUCER	
	SANITARY SEWER	
	STORM SEWER	
	CABLE TV	
	ELECTRIC SERVICE	
	TELEPHONE SERVICE	
	GAS LINE	
	SPOT ELEVATION	
	UTILITY POLE	
	SIGN	
	SANITARY SEWER IDENTIFIER	
	STORM DRAIN IDENTIFIER	
	EASEMENT IDENTIFIER	
	WATER METER	
	FIRE HYDRANT	
	PARKING INDICATOR INDICATES THE NUMBER OF TYPICAL PARKING SPACES	
	STREET LIGHT	
	VEHICLES PER DAY (TRAFFIC COUNT)	
	TEST PIT LOCATION RECOMMENDED/REQUIRED	
	CRITICAL SLOPE SLOPES TO BE STABILIZED PURSUANT TO VIRGINIA EROSION AND SEDIMENT CONTROL HANDBOOK	
	HANDICAP RAMP (CG-12) DENOTES LOCATION OF STD VDOT CG-12 AND/OR JURISDICTIONAL STANDARD RAMP CONSTRUCTION	
	DENOTES CLEAR SIGHT TRIANGLE	
	TREE	
	BENCHMARK	
	ASPHALT TRAIL	
	CONCRETE SIDEWALK	
	END WALLS	
	END SECTIONS	
	STOP SIGN	
	STREET SIGN	
	OVERHEAD ELECTRIC	
	OVERHEAD TELEPHONE	
	HANDICAP PARKING SPACE (VAN)	
	RIP RAP	
	CROSSWALK	

ABBREVIATIONS

A	AREA OF ARC	J	JUNCTION BOX
AD	ALGEBRAIC DIFFERENCE	K	SIGHT DISTANCE COEFFICIENT
AASHTO	AMERICAN ASSOCIATION OF STATE HWY. & TRANSP. OFFICIALS	Ke	CULVERT ENTRANCE LOSS COEFFICIENT
ASTM	AMERICAN SOCIETY FOR TESTING AND MATERIALS	L	LENGTH
AC	ACRE	LAT.	LATERAL
AGGR.	AGGREGATE	LF	LINEAR FOOT
ANSI	AMERICAN NATIONAL STANDARDS INSTITUTE	LL	LOWER LEVEL
ASPH	ASPHALT	LOS	LINE OF SIGHT
AWWA	AMERICAN WATER WORKS ASSOCIATION	LP	LOW POINT
B	BREADTH	LS	LOADING SPACE
BC	BOTTOM OF CURB	LOG	LIMITS OF CLEARING & GRADING
BF	BASEMENT FLOOR	LT	LEFT
BLDG	BUILDING	M	MONUMENT FOUND
BM	BENCHMARK	MECH.	MECHANICAL
BMP	BEST MANAGEMENT PRACTICES (WATER QUALITY)	MH	MANHOLE
BOV	BLOW OFF VALVE	MI	MILE
BRL	BUILDING RESTRICTION LINE	MPH	MILES PER HOUR
BVCS	BEGINNING VERTICAL CURVE STATION	MS	MEDIAN STRIP
BVCE	BEGINNING VERTICAL CURVE ELEVATION	MSL	MEAN SEA LEVEL
BW	BOTTOM OF WALL	MIN	MINIMUM
		MAX	MAXIMUM
C	CENTER CORRECTION ON VERTICAL CURVE	N	NOW OR FORMERLY
C	COEFFICIENT OF RUNOFF	N/F	NET FLOOR AREA
CATV	CABLE TELEVISION	NFA	NUMBER
CB	CATCH BASIN OR CHORD BEARING	NO.#	NORTH BOUND LANE
CC	CENTER TO CENTER	N/A	NOT APPLICABLE
CFS (Q)	CUBIC FEET PER SECOND	OC	ON CENTER
CH	CHORD	OD	OUTSIDE DIAMETER
CG	CURB AND GUTTER	OH	OVERHANG
CIP	CAST IRON PIPE	O/H	OVERHEAD
C	CENTERLINE	P	PERIMETER
CL	CLASS	PC	POINT OF CURVATURE
CMP	CORRUGATED METAL PIPE	PCC	POINT OF COMPOUND CURVE
CONC.	CONCRETE	PCEP	POINT OF CURVE EDGE OF PAVEMENT
CO	CLEAN OUT	PCTC	POINT OF CURVATURE TOP OF CURB
CONT.	CONTINUATION	PFM	PUBLIC FACILITIES MANUAL
CS	CURB STOP	PG	PAGE
CT	COURT	PGL	POINT OF GRADE LINE
C/L	CENTERLINE	PI	POINT OF INTERSECTION
D	DEPTH	PL	PROPERTY LINE
DA	DRAINAGE AREA	PRC	POINT OF REVERSE CURVES
DB	DEED BOOK	PRELIM.	PRELIMINARY
DEQ	VA. DEPARTMENT OF ENVIRONMENTAL QUALITY	PROP.	PROPOSED
DET.	DETAIL	PT	POINT OF TANGENCY
DI	DROP INLET	PVC	POINT OF VERTICAL CURVATURE
DIA.	DIAMETER	PVC	POLY VINYL CHLORIDE
DIP	DUCTILE IRON PIPE	PVI	POINT OF VERTICAL INTERSECTION
DM	DROP MANHOLE	PVM/T	PAVEMENT
DR	DRIVE	PVRC	POINT OF VERTICAL REVERSE CURVE
DRNG	DRAINAGE	PVT	POINT OF VERTICAL TANGENT
DRWG.	DRAWING	P&P	PLAN AND PROFILE
D/W	DRIVE WAY	Q	Q(C.F.S.) AMOUNT OF RUNOFF
Δ	DELTA	R	RADIUS
DU	DWELLING UNITS	REQD	REQUIRED
DOM	DOMESTIC	REOD	REINFORCED CONCRETE PIPE
		RD	ROAD
E	RATE OF SUPER ELEVATION IN FEET PER FOOT	RET.	RETAINING
EC	EROSION CONTROL	REV.	REVISION
EGL	ENERGY GRADIENT LINE	RR	RAILROAD
EQC	ENVIRONMENTAL QUALITY CORRIDOR	RTE.	ROUTE
ESM/T	EASEMENT	R/W	RIGHT OF WAY
EG	EDGE OF GUTTER	RGP	ROUGH GRADING PLAN
ELEV.	ELEVATION	ROM	REMOTE OUTSIDE MONITOR
ENT.	ENTRANCE	RMA	RESOURCE MANAGEMENT AREA
EP	EDGE OF PAVEMENT	RPA	RESOURCE PROTECTION AREA
ES	END SECTION	RT	RIGHT
EVCS	ENDING VERTICAL CURVE STATION	S	SPEED OR SLOPE
EVCE	ENDING VERTICAL CURVE ELEVATION	SAN.	SANITARY
EW	END WALL	SBL	SOUTH BOUND LANE
EX.	EXISTING	SD	SIGHT DISTANCE
ELEC.	ELECTRICAL	SECT.	SECTION
EBL	EAST BOUND LANE	SEW.	SEWER
		SH.	SHOULDER
F	FIRE LINE	SF	SQUARE FEET
FAR	FLOOR AREA RATIO	SP.	SPACE
FC	FACE OF CURB	SP	SITE PLAN
FSWA	FREDERICKSBURG SEWER AND WATER AUTHORITY	SPEC.	SPECIFICATION
FF	FIRST FLOOR	STA.	STATION
FG	FINISHED GRADE	STD.	STANDARD
FH	FIRE HYDRANT	STK.	STACK
FL	FLOW LINE	STM.	STORM
FP	FLOOD PLAIN	SVC.	SERVICE
FS	FACTOR OF SAFETY	SWM	STORM WATER MANAGEMENT
FT.	FOOT	S/W	SIDE WALK
FSPA	FREDERICKSBURG-STAFFORD PARK AUTHORITY	Sx	CROSS SLOPE
FOY.	FOYER	I	TANGENT
FPS	FEET PER SECOND	TB	TEST BORE
		TC	TOP OF CURB
G	GRAVITY	Tc	TIME OF CONCENTRATION
GAS	GAS	TEL	TELEPHONE
GFA	GROSS FLOOR AREA	TP	TEST PIT
G	GRADE	TP	TREE PROTECTION
GR	GUARD RAIL	TB	TOP OF BANK
GAR	GARAGE	TW	TOP OF WALL
		TW	TAILWATER
H	HEIGHT	UD	UNDERDRAIN
H	HEAD	UG	UNDERGROUND
HC	HANDICAPPED PARKING SPACE	UL	UPPER LEVEL
HGL	HYDRAULIC GRADIENT LINE	UP	UTILITY POLE
HP	HIGH POINT	VAN	HANDICAPPED VAN PARKING SPACE
HR	HAND RAIL	V	VELOCITY
HT.	HEIGHT	V	VOLUME
HW	HEADWATER	VA	VIRGINIA
		VC	VERTICAL CURVE
I	RAINFALL INTENSITY	VDOT	VA. DEPT. OF TRANSPORTATION
ID	INSIDE DIAMETER	VF	VERTICAL FOOT
IN	INCH	W	WEIGHT OR WIDTH
INV.	INVERT	W/M	WATER MAIN
IP	IRON PIPE	WBL	WEST BOUND LANE
IPF	IRON PIPE FOUND	WQIA	WATER QUALITY IMPACT ASSESSMENT
IPS	IRON PIPE SET	X	TRANSFORMER
		Y	YARD INLET
		YR	YEAR
		Z	SIDE SLOPES



NOTES

1. THIS REZONING PLAT IS SUBJECT TO ALL COVENANTS AND RESTRICTIONS OF RECORD AND THOSE RECORDED HERewith. BOWMAN CONSULTING GROUP, LTD. WAS PROVIDED A COMMITMENT FOR TITLE INSURANCE FROM BRIDGE TRUST TITLE GROUP, FORMERLY PIONEER TITLE, FILE NO. 091045098, COMMITMENT DATE: APRIL 23, 2014, 8:00 AM, ISSUE DATE: MAY 02, 2014, 10:45 AM.
2. THE PROPERTIES DELINEATED HEREON CURRENTLY STAND IN THE NAMES OF GLORIA B. WHITTAKER, BONNIE B. CARTER AND CAROL B. COLEMAN AS RECORDED IN DEED BOOK 208 AT PAGE 100 AND DEED BOOK 208 AT PAGE 97 IN THE CLERK'S OFFICE OF THE CITY OF FREDERICKSBURG AND IS SHOWN AS A PORTION OF GPIN: 7769-87-3295 AND GPIN: 7769-77-8378.
3. BOUNDARY AND MERIDIAN SHOWN HEREON ARE BASED ON A PLAT ENTITLED "PLAT OF BOUNDARY AND TOPOGRAPHIC SURVEY TAX MAP 272 PARCEL 91" PREPARED BY BAGBY, CALDWELL AND ASSOCIATES, P.C., DATED FEBRUARY 2005 AND DOES NOT REPRESENT A CURRENT FIELD RUN SURVEY BY BOWMAN CONSULTING.
4. THE PROPERTY SHOWN HEREON LIES WITHIN FLOOD ZONE "X" (AREA DETERMINED TO BE OUTSIDE 500-YEAR FLOOD PLAIN) AS DESIGNATED ON F.I.R.M. COMMUNITY-PANEL NO. 510308 0075 B.
5. THIS PLAT HAS BEEN REVISED TO REFLECT RIGHT-OF-WAY DEDICATION ALONG FALL HILL AVENUE RECORDED AT INSTRUMENT 150001796 AMONG THE LAND RECORDS OF THE CITY OF FREDERICKSBURG.

LEGEND

	DENOTES WELL
	DENOTES FIRE HYDRANT
	DENOTES POLE
	DENOTES SIGN
	DENOTES FENCE
	DENOTES TREE OR TREELINE
	DENOTES BRUSHLINE
	DENOTES TRAILER OR OUT-BUILDINGS

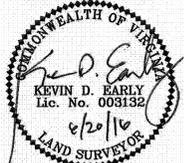
REZONING PLAT
ON A PORTION OF
GPIN: 7769-87-3295 AND GPIN: 7769-77-8378
DEED BOOK 208, PAGE 97 & 100
CITY OF FREDERICKSBURG, VIRGINIA

SCALE: 1" = 100' DATE: JUNE 27, 2014

REVISION	DATE	DESCRIPTION
6/1/16	REVISED PER DEDICATION	
6/20/16	REVISED BRISCOE	

Bowman CONSULTING
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DWG: P:\9432\9432-01-001 (SUR)\Survey\Plats\9432-C-MP-001.dwg BY: JA CHK: DLD QC:
BCG PROJECT NO: TASK: COUNTY REF NO: TM 272, PCL 91 SHEET 1 OF 1



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REZONING PLAT
THE HAMPTONS PHASE II
GENERAL DEVELOPMENT PLAN

CITY OF FREDERICKSBURG, VIRGINIA WARD 1

CITY PROJECT NUMBER

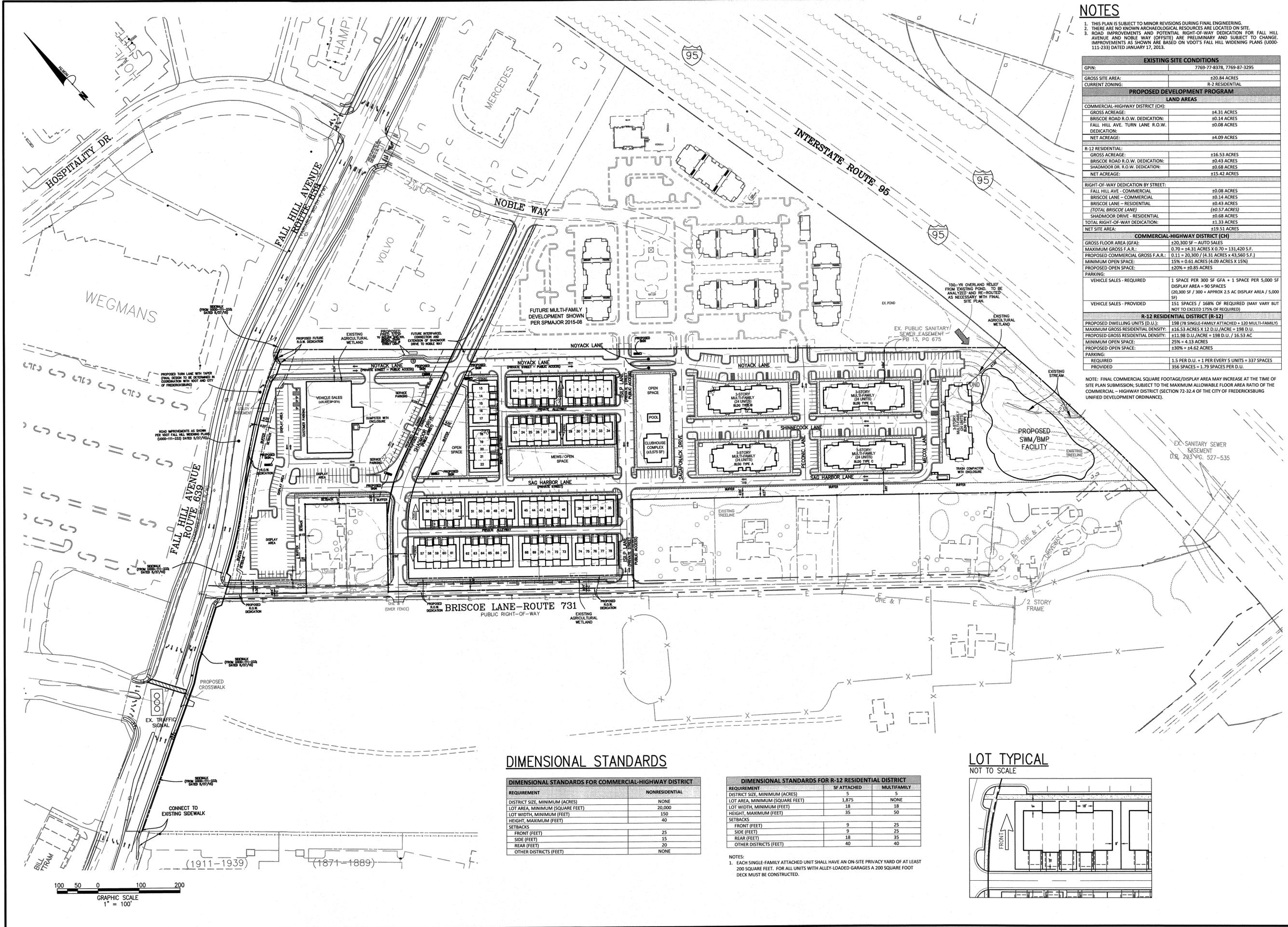


PLAN STATUS

03/31/16	PRE-APP SUBMISSION
06/20/16	1ST SUBMISSION
08/09/16	2ND SUBMISSION
10/11/16	ADDRESS COMMENTS
10/19/16	ADDRESS COMMENTS

DATE	DESCRIPTION
BO/SG	WSP
DESIGN	SG
SCALE	H: AS NOTED
V:	
JOB No.	9432-01-001
DATE :	MAY 2016
FILE No.	9432-D-ZP-001

SEE SHEET 5 FOR ADJACENT PROPERTY OWNERS



NOTES

- THIS PLAN IS SUBJECT TO MINOR REVISIONS DURING FINAL ENGINEERING.
- THERE ARE NO KNOWN ARCHAEOLOGICAL RESOURCES LOCATED ON SITE.
- ROAD IMPROVEMENTS AND POTENTIAL RIGHT-OF-WAY DEDICATION FOR FALL HILL AVENUE AND NOBLE WAY (OFFSITE) ARE PRELIMINARY AND SUBJECT TO CHANGE. IMPROVEMENTS AS SHOWN ARE BASED ON VDOT'S FALL HILL WIDENING PLANS (U000-111-233) DATED JANUARY 17, 2013.

EXISTING SITE CONDITIONS	
GPIN:	7769-77-8378, 7769-87-3295
GROSS SITE AREA:	±20.84 ACRES
CURRENT ZONING:	R-2 RESIDENTIAL
PROPOSED DEVELOPMENT PROGRAM	
LAND AREAS	
COMMERCIAL-HIGHWAY DISTRICT (CH):	
GROSS ACREAGE:	±4.31 ACRES
BRISCOE ROAD R.O.W. DEDICATION:	±0.14 ACRES
FALL HILL AVE. TURN LANE R.O.W. DEDICATION:	±0.08 ACRES
NET ACREAGE:	±4.09 ACRES
R-12 RESIDENTIAL:	
GROSS ACREAGE:	±16.53 ACRES
BRISCOE ROAD R.O.W. DEDICATION:	±0.43 ACRES
SHADMOOR DR. R.O.W. DEDICATION:	±0.68 ACRES
NET ACREAGE:	±15.42 ACRES
RIGHT-OF-WAY DEDICATION BY STREET:	
FALL HILL AVE. - COMMERCIAL:	±0.08 ACRES
BRISCOE LANE - COMMERCIAL:	±0.14 ACRES
BRISCOE LANE - RESIDENTIAL:	±0.43 ACRES
(TOTAL BRISCOE LANE):	(±0.57 ACRES)
SHADMOOR DRIVE - RESIDENTIAL:	±0.68 ACRES
TOTAL RIGHT-OF-WAY DEDICATION:	±1.33 ACRES
NET SITE AREA:	±19.51 ACRES
COMMERCIAL-HIGHWAY DISTRICT (CH)	
GROSS FLOOR AREA (GFA):	±20,300 SF - AUTO SALES
MAXIMUM GROSS F.A.R.:	0.70 = ±4.31 ACRES X 0.70 = 131,420 S.F.
PROPOSED COMMERCIAL GROSS F.A.R.:	0.11 = 20,300 / (4.31 ACRES X 43,560 S.F.)
MINIMUM OPEN SPACE:	15% = 0.61 ACRES (4.09 ACRES X 15%)
PROPOSED OPEN SPACE:	±20% = ±0.85 ACRES
PARKING:	
VEHICLE SALES - REQUIRED:	1 SPACE PER 300 SF GFA + 1 SPACE PER 5,000 SF DISPLAY AREA = 90 SPACES (20,300 SF / 300 + APPROX 2.5 AC DISPLAY AREA / 5,000 SF)
VEHICLE SALES - PROVIDED:	151 SPACES / 168% OF REQUIRED (MAY VARY BUT NOT TO EXCEED 175% OF REQUIRED)
R-12 RESIDENTIAL DISTRICT (R-12)	
PROPOSED DWELLING UNITS (D.U.):	198 (78 SINGLE-FAMILY ATTACHED + 120 MULTI-FAMILY)
MAXIMUM GROSS RESIDENTIAL DENSITY:	±16.53 ACRES X 12 D.U./ACRE = 198 D.U.
PROPOSED GROSS RESIDENTIAL DENSITY:	±11.98 D.U./ACRE = 198 D.U. / 16.53 AC
MINIMUM OPEN SPACE:	25% = 4.13 ACRES
PROPOSED OPEN SPACE:	±30% = ±4.62 ACRES
PARKING:	
REQUIRED:	1.5 PER D.U. + 1 PER EVERY 5 UNITS = 337 SPACES
PROVIDED:	356 SPACES = 1.79 SPACES PER D.U.

NOTE: FINAL COMMERCIAL SQUARE FOOTAGE/DISPLAY AREA MAY INCREASE AT THE TIME OF SITE PLAN SUBMISSION; SUBJECT TO THE MAXIMUM ALLOWABLE FLOOR AREA RATIO OF THE COMMERCIAL - HIGHWAY DISTRICT (SECTION 72-24.4 OF THE CITY OF FREDERICKSBURG UNIFIED DEVELOPMENT ORDINANCE).

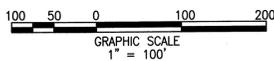
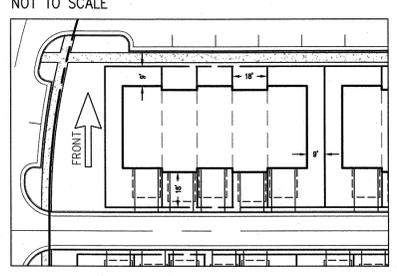
DIMENSIONAL STANDARDS

REQUIREMENT	NONRESIDENTIAL
DISTRICT SIZE, MINIMUM (ACRES)	NONE
LOT AREA, MINIMUM (SQUARE FEET)	20,000
LOT WIDTH, MINIMUM (FEET)	150
HEIGHT, MAXIMUM (FEET)	40
SETBACKS	
FRONT (FEET)	25
SIDE (FEET)	15
REAR (FEET)	20
OTHER DISTRICTS (FEET)	NONE

REQUIREMENT	SF ATTACHED	MULTIFAMILY
DISTRICT SIZE, MINIMUM (ACRES)	5	5
LOT AREA, MINIMUM (SQUARE FEET)	1,875	NONE
LOT WIDTH, MINIMUM (FEET)	18	18
HEIGHT, MAXIMUM (FEET)	35	50
SETBACKS		
FRONT (FEET)	9	25
SIDE (FEET)	9	25
REAR (FEET)	18	35
OTHER DISTRICTS (FEET)	40	40

NOTES:
1. EACH SINGLE-FAMILY ATTACHED UNIT SHALL HAVE AN ON-SITE PRIVACY YARD OF AT LEAST 200 SQUARE FEET. FOR ALL UNITS WITH ALLEY-LOADED GARAGES A 200 SQUARE FOOT DECK MUST BE CONSTRUCTED.

LOT TYPICAL



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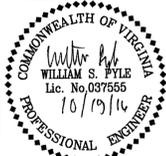
**SCHEMATIC PLAN
THE HAMPTONS PHASE II
GENERAL DEVELOPMENT PLAN**

CITY PROJECT NUMBER



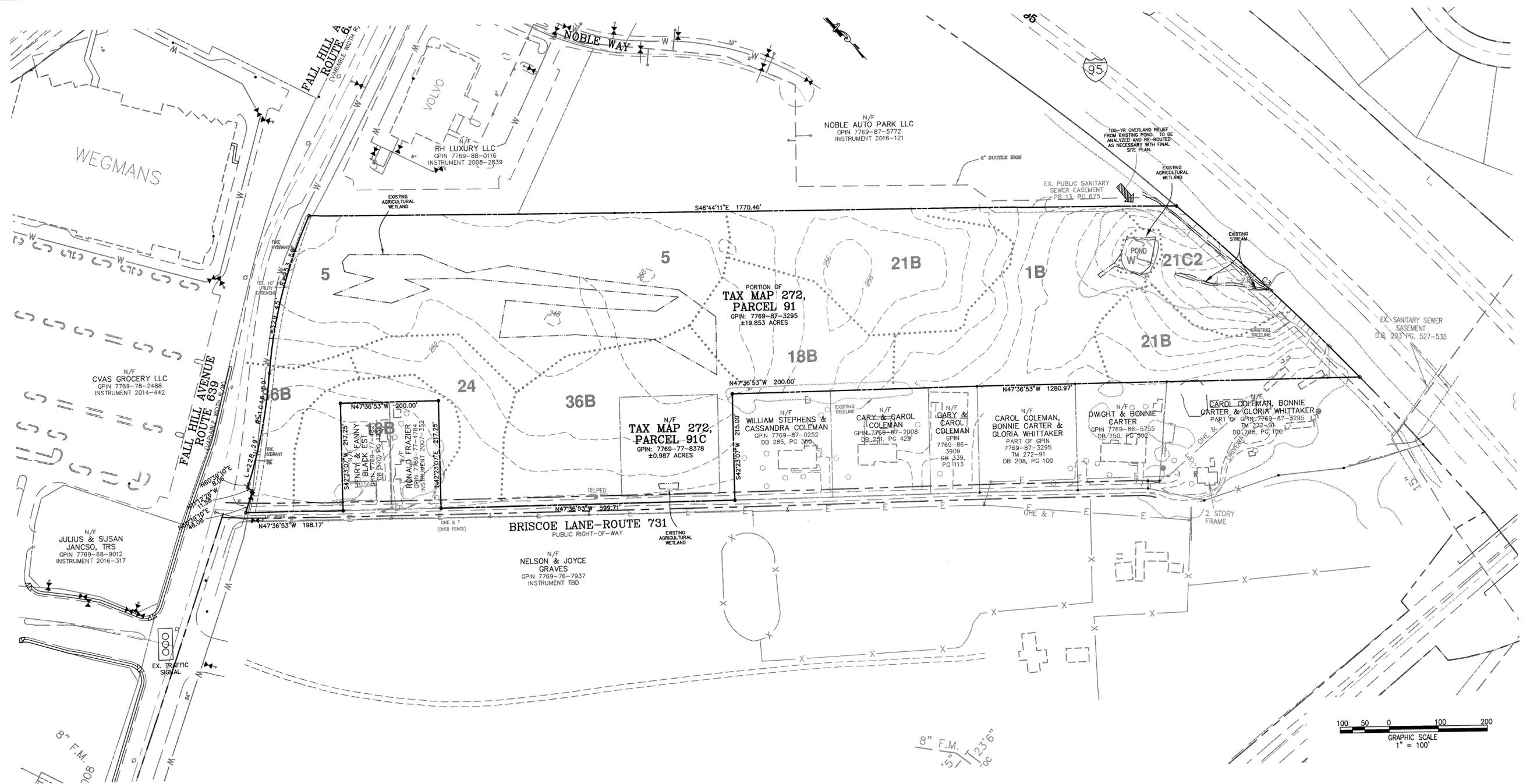
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03/31/16	PRE-APP SUBMISSION
06/20/16	1ST SUBMISSION
08/09/16	2ND SUBMISSION
10/11/16	ADDRESS COMMENTS
10/19/16	ADDRESS COMMENTS

DATE	DESCRIPTION
BO/SG	SG WSP
DESIGN	DRAWN CHKD
SCALE	H: 1"=100'
	V:
JOB No.	9432-01-001
DATE :	MAY 2016
FILE No.	9432-D-ZP-001



PLAN STATUS	
03/31/16	PRE-APP SUBMISSION
06/20/16	1ST SUBMISSION
08/09/16	2ND SUBMISSION
10/11/16	ADDRESS COMMENTS
10/19/16	ADDRESS COMMENTS

DATE	DESCRIPTION
BO/SG	SG WSP
DESIGN	DRAWN CHKD
SCALE	H: 1"=100'
	V:
JOB No.	9432-01-001
DATE	MAY 2016
FILE No.	9432-D-ZP-001

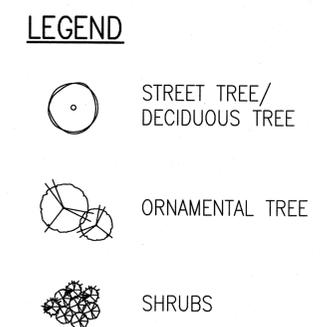
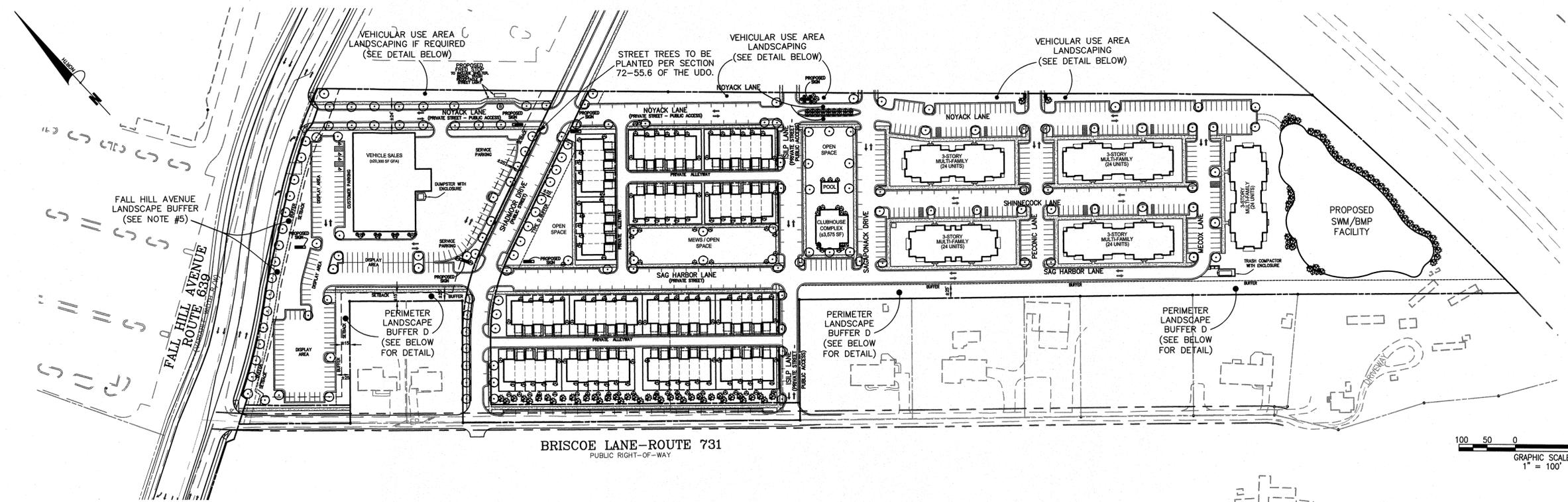


NOTES

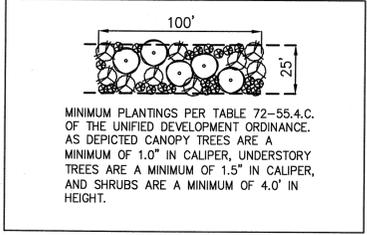
- WETLAND AREAS HAVE BEEN PRELIMINARILY FIELD LOCATED AND ARE DEPICTED ON THIS PLAN. THESE WETLAND AREAS ARE LOCATED ON EXISTING AGRICULTURAL LAND AND A JURISDICTIONAL DETERMINATION IS CURRENTLY BEING PROCESSED WITH THE ARMY CORPS OF ENGINEERS. PER CITY OF FREDERICKSBURG INFORMATION THE SITE HAS BEEN IDENTIFIED AS BEING WITHIN THE CHESAPEAKE BAY PRESERVATION OVERLAY DISTRICT.
- THE PROPERTY IS LOCATED IN ZONE "X" (AREAS DETERMINED TO BE OUTSIDE 500 YEAR FLOODPLAIN) AS SHOWN ON FEMA FLOOD INSURANCE RATE MAPS FOR THE CITY OF FREDERICKSBURG, VIRGINIA, COMMUNITY-PANEL NUMBER 510308 0075B.
- THERE ARE NO KNOWN CEMETERIES OR HISTORIC SITES LOCATED ON THE SUBJECT PROPERTY.
- THERE ARE NO SLOPES IN EXCESS OF 20 PERCENT LOCATED ON THE SUBJECT PROPERTY.

SOIL TYPES

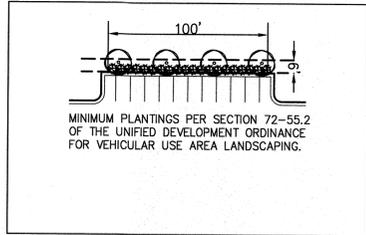
MAP UNIT SYMBOL	MAP UNIT NAME
1B	ABELL SANDY LOAM
5	AQUULTS, CLAYEY SUBSOIL
18B	EMPORIA SANDY LOAM
21B	FACEVILLE-VARINA COMPLEX
21C2	FACEVILLE-VARINA COMPLEX
24	GOLDSBORO SANDY LOAM
36B	SAVANNAH SANDY LOAM
W	WATER



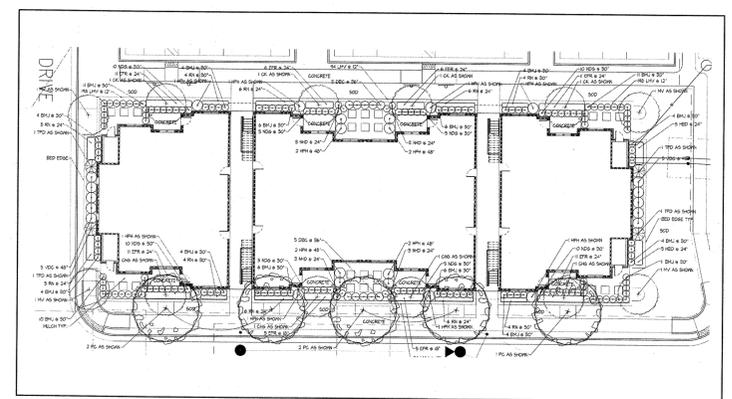
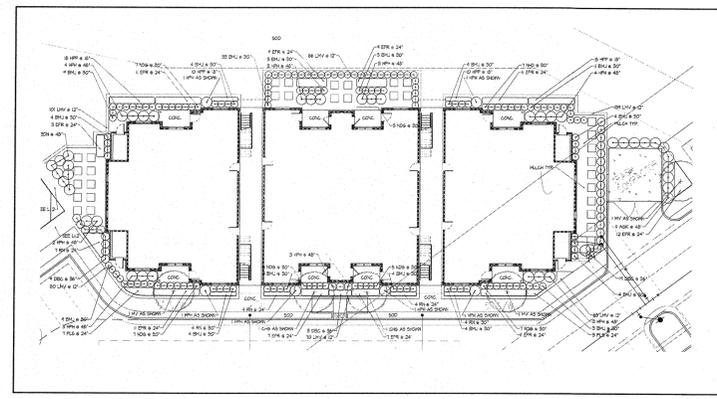
TYPE D PERIMETER LANDSCAPE BUFFER DETAIL (TYPICAL)
 NOT TO SCALE



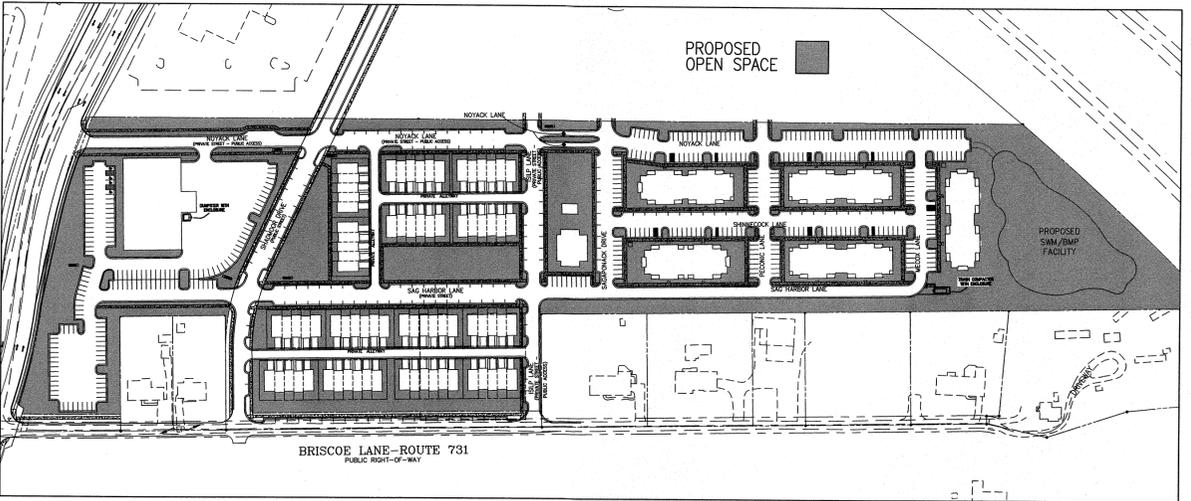
VEHICULAR USE AREA LANDSCAPING DETAIL (TYPICAL)
 NOT TO SCALE



MULTI-FAMILY LANDSCAPING (TYPICAL)
 NOT TO SCALE

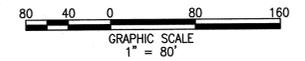
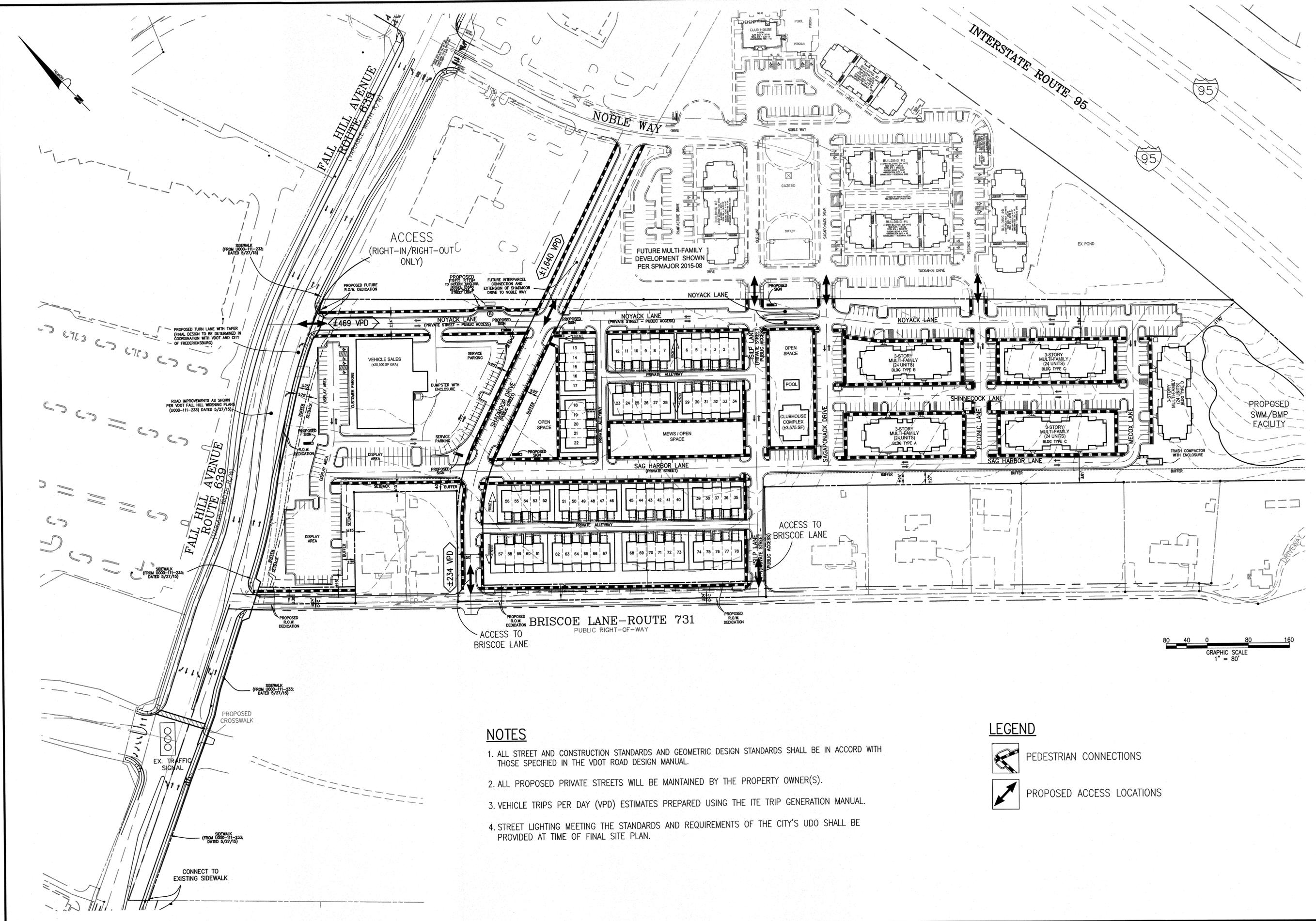


PROPOSED OPEN SPACE
 NOT TO SCALE



NOTES

1. THIS PLAN IS CONCEPTUAL IN NATURE AND SUBJECT TO MINOR REVISION WITH FINAL ENGINEERING.
2. ALL LANDSCAPING SHALL BE PLANTED IN ACCORDANCE WITH SECTION 72-55 OF THE CITY OF FREDERICKSBURG UNIFIED DEVELOPMENT ORDINANCE (UDO); UNLESS WAIVED OR MODIFIED.
3. ALL LANDSCAPING SHALL BE MAINTAINED BY THE PROPERTY OWNER(S) IN ACCORDANCE WITH THE UDO.
4. ALL SIGNS (BUILDING AND FREE-STANDING) SHALL BE ERECTED IN ACCORDANCE WITH SECTION 72-59 OF THE UDO. MORE SPECIFICALLY, NO SIGN SHALL BE ERECTED UNTIL A SIGN DESIGN PACKAGE HAS BEEN SUBMITTED AND APPROVED BY THE DEVELOPMENT ADMINISTRATOR IN ACCORDANCE WITH SECTION 72-59.6.C OF THE UDO. ADDITIONAL SIGNS MAY BE PROPOSED IN THE FUTURE SUBJECT TO COMPLIANCE WITH THE UDO AND AN APPROVED SIGN DESIGN PACKAGE.
5. THE FALL HILL AVENUE BUFFER SHALL BE 20 FEET WIDE AND GENERALLY CONSIST OF A MEANDERING BERM THAT SHALL BE A MINIMUM OF 2.5 FEET HIGH. LANDSCAPING SHALL CONSIST OF TREES PLANTED THIRTY (30) FEET ON CENTER WITH A MINIMUM D.B.H. OF 3.5 INCHES. IN ADDITION, FOUR (4) ORNAMENTAL SHRUBS SHALL BE PLANTED BETWEEN EACH THIRTY (30) TREE SECTION. SEE PROFFER STATEMENT FOR ADDITIONAL INFORMATION.
6. STREET TREES MEETING THE STANDARDS & REQUIREMENTS OF THE CITY'S UDO SHALL BE PROVIDED AT THE TIME OF FINAL SITE PLAN.



NOTES

1. ALL STREET AND CONSTRUCTION STANDARDS AND GEOMETRIC DESIGN STANDARDS SHALL BE IN ACCORD WITH THOSE SPECIFIED IN THE VDOT ROAD DESIGN MANUAL.
2. ALL PROPOSED PRIVATE STREETS WILL BE MAINTAINED BY THE PROPERTY OWNER(S).
3. VEHICLE TRIPS PER DAY (VPD) ESTIMATES PREPARED USING THE ITE TRIP GENERATION MANUAL.
4. STREET LIGHTING MEETING THE STANDARDS AND REQUIREMENTS OF THE CITY'S UDO SHALL BE PROVIDED AT TIME OF FINAL SITE PLAN.

LEGEND

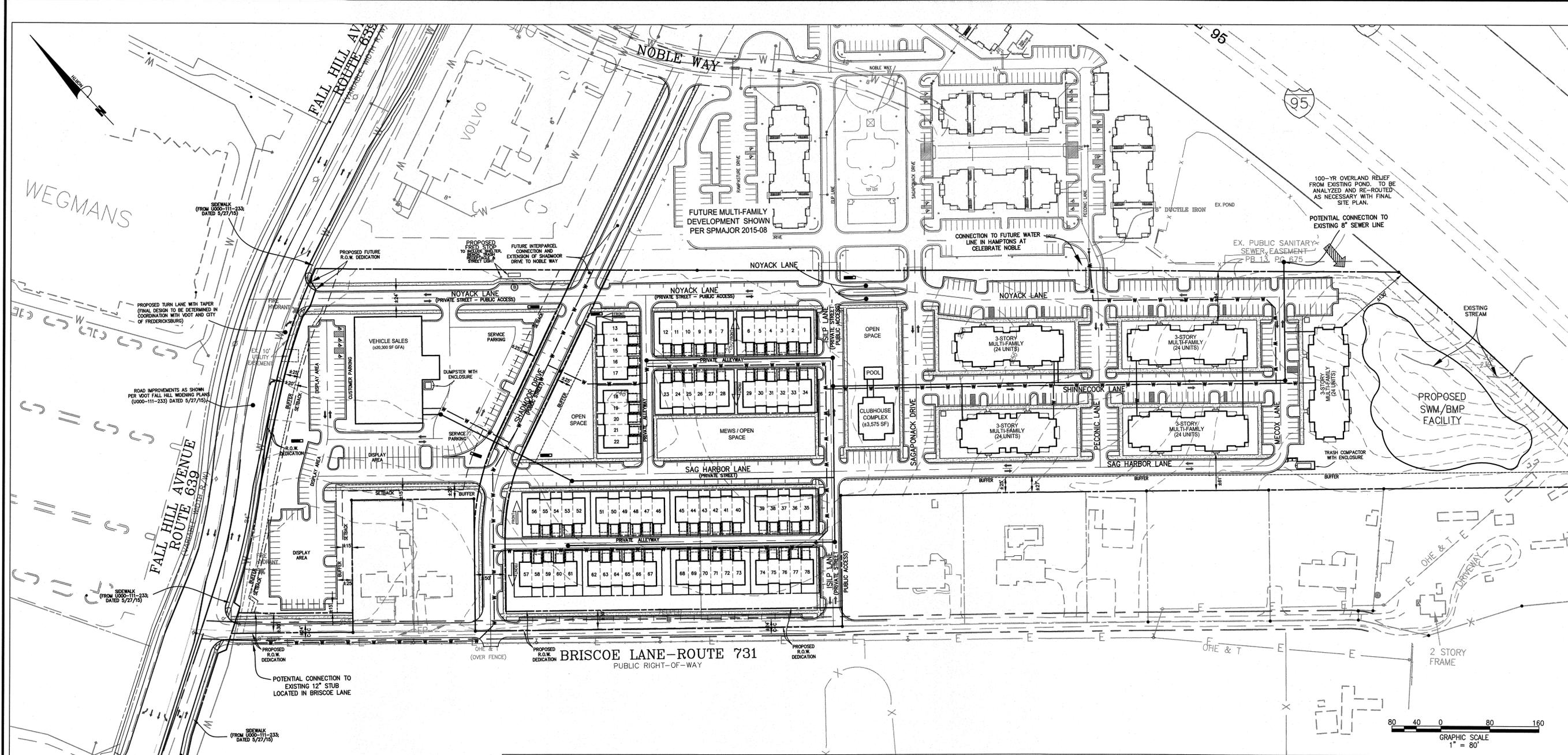
- PEDESTRIAN CONNECTIONS
- PROPOSED ACCESS LOCATIONS

CITY PROJECT NUMBER



PLAN STATUS		
03/31/16	PRE-APP SUBMISSION	
06/20/16	1ST SUBMISSION	
08/09/16	2ND SUBMISSION	
10/11/16	ADDRESS COMMENTS	
10/19/16	ADDRESS COMMENTS	

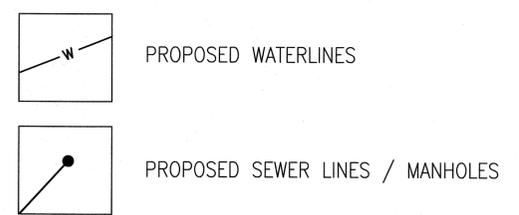
DATE	DESCRIPTION	
BO/SG	SG	WSP
DESIGN	DRAWN	CHKD
SCALE	H: 1"=80'	
	V:	
JOB No. 9432-01-001		
DATE : MAY 2016		
FILE No. 9432-D-ZP-001		



NOTES

1. THIS PLAN IS CONCEPTUAL IN NATURE AND IS INTENDED TO DEMONSTRATE A POTENTIAL SEWER AND WATER UTILITY LAYOUT. REVISION TO THIS PLAN MAY BE REQUIRED WITH FINAL ENGINEERING.
2. ALL UTILITY CONSTRUCTION STANDARDS AND GEOMETRIC DESIGN STANDARDS SHALL BE IN ACCORD WITH THOSE SPECIFIED BY THE CITY OF FREDERICKSBURG.
3. THE PROPOSED DEVELOPMENT IS NOT ANTICIPATED TO HAVE A SIGNIFICANT IMPACT ON THE PUBLIC WATER SYSTEM. THE POTENTIAL CONNECTIONS WILL IMPROVE SYSTEM PRESSURE AND REDUNDANCY.
4. THE PROPOSED DEVELOPMENT IS NOT ANTICIPATED TO HAVE A SIGNIFICANT IMPACT ON THE PUBLIC SANITARY SEWER SYSTEM. THE PROJECT WILL CONNECT TO AN EXISTING 8" SEWER MAIN LOCATED ALONG THE PROPERTY'S SOUTHERN BOUNDARY AND THEN TIE INTO AN EXISTING 15" SEWER MAIN. ADDITIONAL STUDY MAY BE REQUIRED TO ASCERTAIN THE ADEQUACY OF CAPACITY AND OUTFALL.
5. STORMWATER AND BMP DESIGN WILL BE IN ACCORDANCE WITH ALL STATE AND CITY REQUIREMENTS. A POTENTIAL STORM POND IS PROPOSED NEXT TO THE INTERSTATE 95 RIGHT-OF-WAY. THIS FACILITY WILL SERVE THE SITE AND STORMWATER GENERATED FROM FALL HILL AVENUE. COORDINATION WITH VDOT WILL BE NECESSARY AS FINAL PLANS ARE COMPLETED FOR THE IMPROVEMENTS TO FALL HILL AVENUE. ALL NECESSARY BMP TREATMENT OF STORMWATER SHALL OCCUR ON THE SUBJECT PROPERTY.
6. SEE THE APPLICATION NARRATIVE FOR FURTHER DISCUSSION OF IMPACTS TO PUBLIC FACILITIES.

LEGEND



CITY PROJECT NUMBER



PLAN STATUS

03/31/16	PRE-APP SUBMISSION
06/20/16	1ST SUBMISSION
08/09/16	2ND SUBMISSION
10/11/16	ADDRESS COMMENTS
10/19/16	ADDRESS COMMENTS

DATE	DESCRIPTION
BO/SG DESIGN	SG WSP DRAWN CHKD
SCALE	H: N/A V:
JOB No.	9432-01-001
DATE	MAY 2016
FILE No.	9432-D-ZP-001

EXHIBIT B
ELEVATIONS

7978182-1 013680.02753



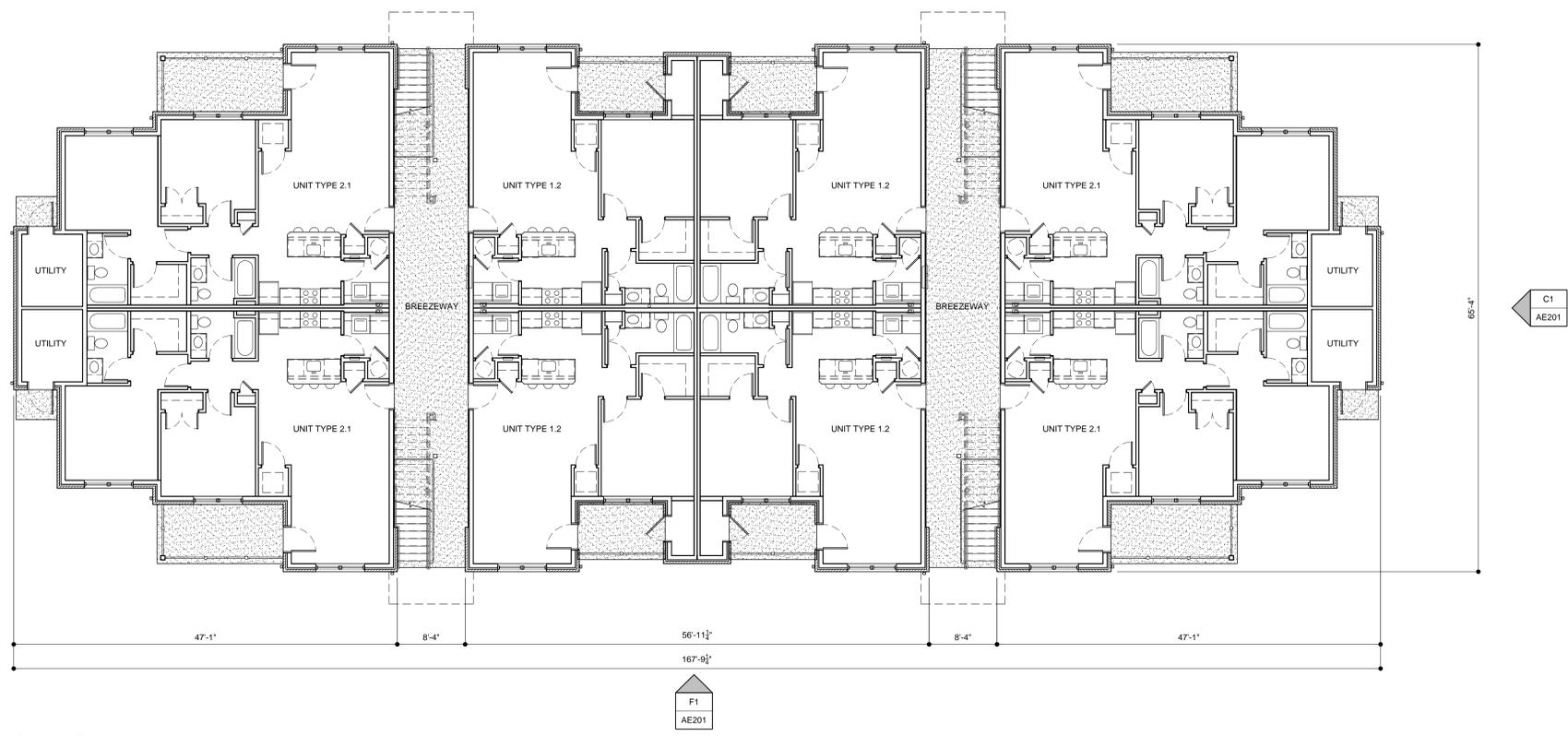
BUILDING TYPE D - FIRST FLOOR PLAN

SCALE: 1/8" = 1'-0"

F1
AD201

	1789 LONDON BRIDGE ROAD SUITE 201 VIRGINIA BEACH, VIRGINIA 23463 www.ts3architects.com 757.689.2899	PROJECT TITLE THE HAMPTONS PHASE II
	TS3 ARCHITECTS+PC © 2016 VIRGINIA CORPORATION NO. 408001837	PROJECT NUMBER TS316041.00 CONTRACT DRAWING DATE JUNE 17, 2016
CONSULTANTS	REVISIONS / SUBMISSIONS	DRAWING TITLE BUILDING TYPE D - FIRST FLOOR PLAN DRAWING NUMBER AD101 SHEET 1 OF 11

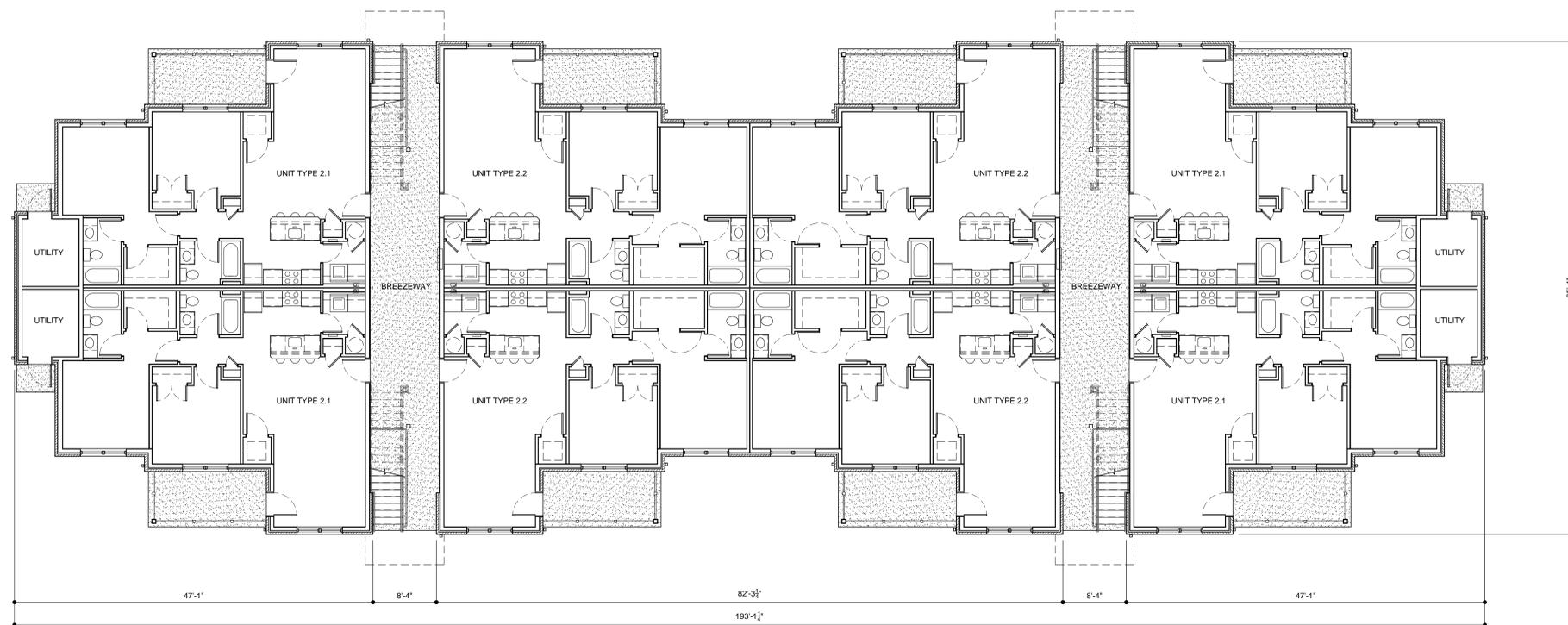
M
G
F
E
D
C
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A



BUILDING TYPE E - FIRST FLOOR PLAN

SCALE: 1/8" = 1'-0"

	1780 LONDON BRIDGE ROAD SUITE 201 VIRGINIA BEACH, VIRGINIA 23463 www.ts3architects.com 757.689.2899	PROJECT TITLE THE HAMPTONS PHASE II
	TS3 ARCHITECTS+PC © 2016 VIRGINIA CORPORATION NO. 408001837	PROJECT NUMBER TS316041.00 CONTRACT DRAWING DATE JUNE 17, 2016
CONSULTANTS	REVISIONS / SUBMISSIONS	DRAWING TITLE BUILDING TYPE E - FIRST FLOOR PLAN
		DRAWING NUMBER AE101
		SHEET 1 OF 11



BUILDING TYPE F - FIRST FLOOR PLAN

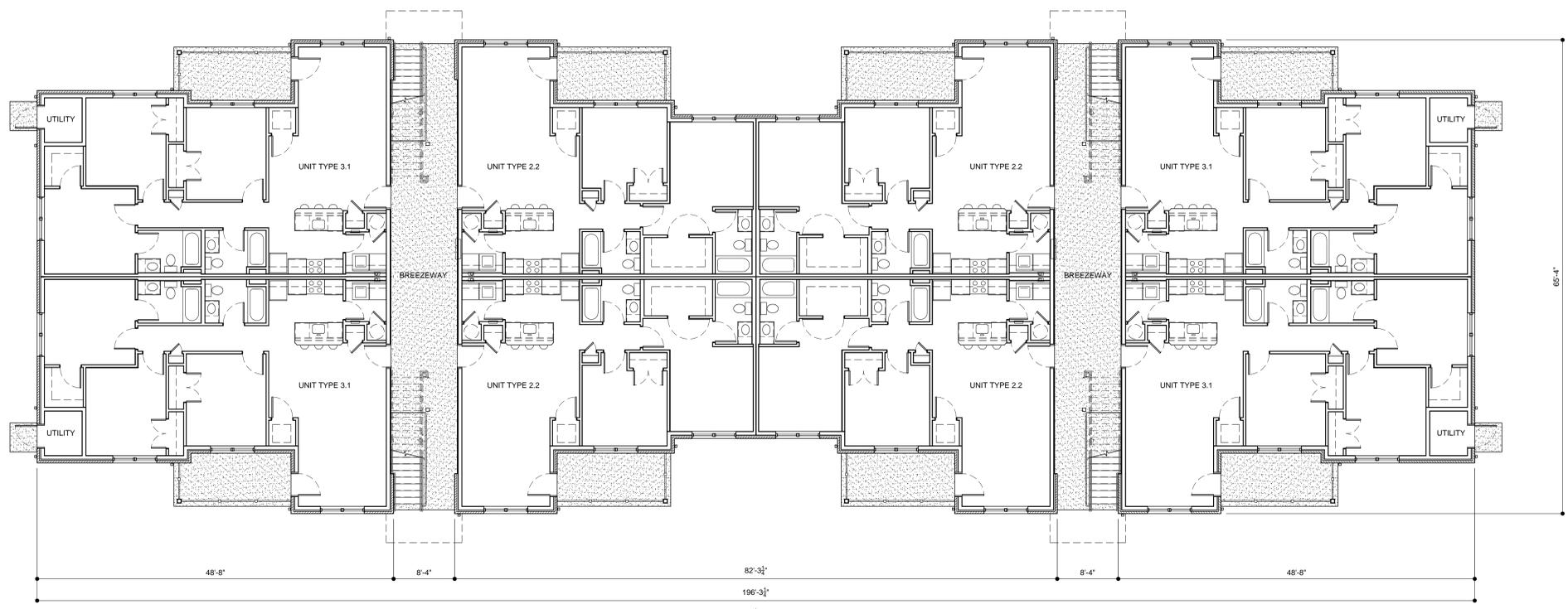
SCALE: 1/8" = 1'-0"

F1
AF201

C1
AF201

 <p>CONSULTANTS</p>	<p>1789 LONDON BRIDGE ROAD SUITE 201 VIRGINIA BEACH, VIRGINIA 23453 www.ts3architects.com 757.689.2899</p>	<p>PROJECT TITLE THE HAMPTONS PHASE II</p>
	<p>TS3 ARCHITECTS+PC © 2016 VIRGINIA CORPORATION NO. 408001837</p>	<p>PROJECT NUMBER TS316041.00</p>
<p>REVISIONS / SUBMISSIONS</p>	<p>CONTRACT DRAWING DATE JUNE 17, 2016</p>	<p>DRAWING TITLE BUILDING TYPE F - FIRST FLOOR PLAN</p>
<p>DRAWING NUMBER</p>		<p>AF101</p>
<p>SHEET 1 OF 11</p>		

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C1
AH201

BUILDING TYPE H - FIRST FLOOR PLAN

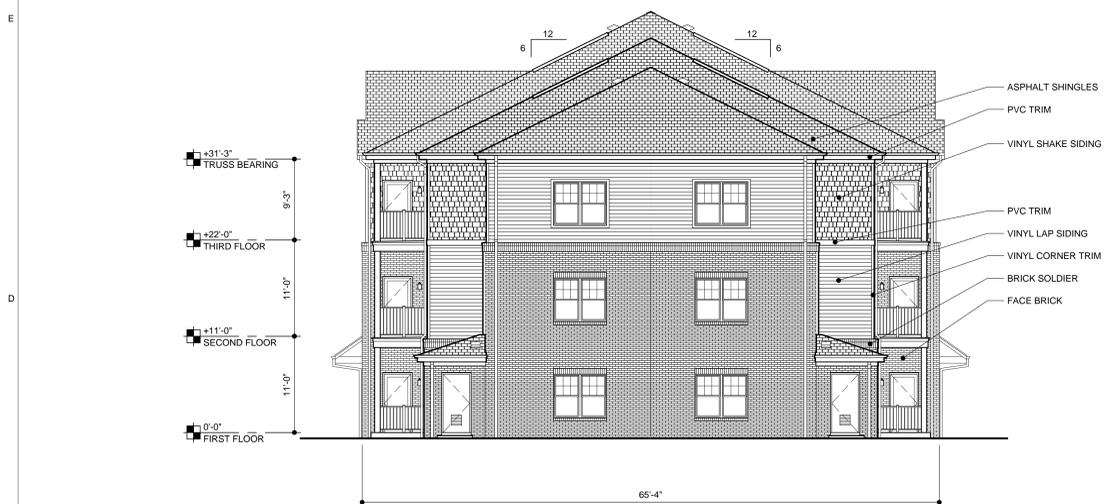
SCALE: 1/8" = 1'-0"

F1
AH201

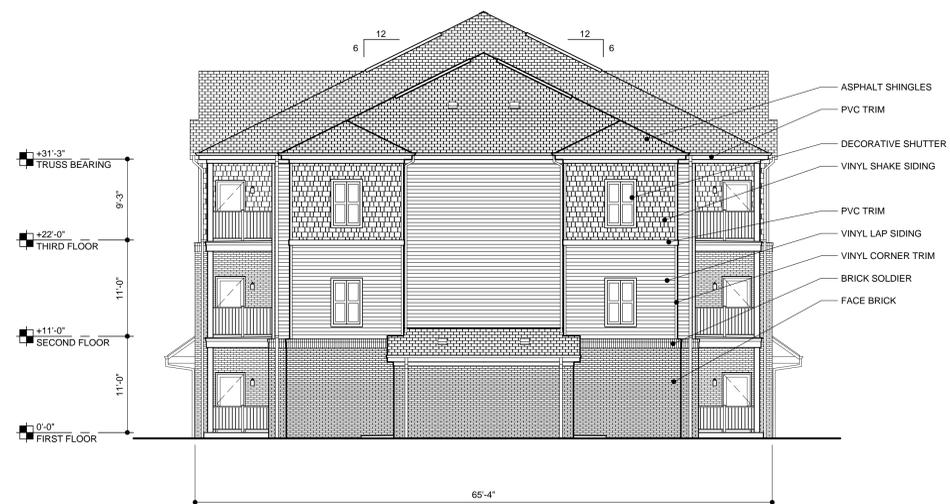
	1789 LONDON BRIDGE ROAD SUITE 201 VIRGINIA BEACH, VIRGINIA 23453 www.ts3architects.com 757.689.2899	PROJECT TITLE THE HAMPTONS PHASE II
	TS3 ARCHITECTS+PC © 2016 VIRGINIA CORPORATION NO. 408001837	PROJECT NUMBER TS316041.00 CONTRACT DRAWING DATE JUNE 17, 2016
CONSULTANTS	REVISIONS / SUBMISSIONS	DRAWING TITLE BUILDING TYPE H - FIRST FLOOR PLAN
		DRAWING NUMBER AH101
		SHEET 1 OF 11



F1 BUILDING TYPE D - EXTERIOR ELEVATION
AD201 SCALE: 1/8" = 1'-0"



C1 BUILDING TYPE D - EXTERIOR ELEVATION
AD201 SCALE: 1/8" = 1'-0"



C5 BUILDING TYPE D - EXTERIOR ELEVATION
AD201 SCALE: 1/8" = 1'-0"

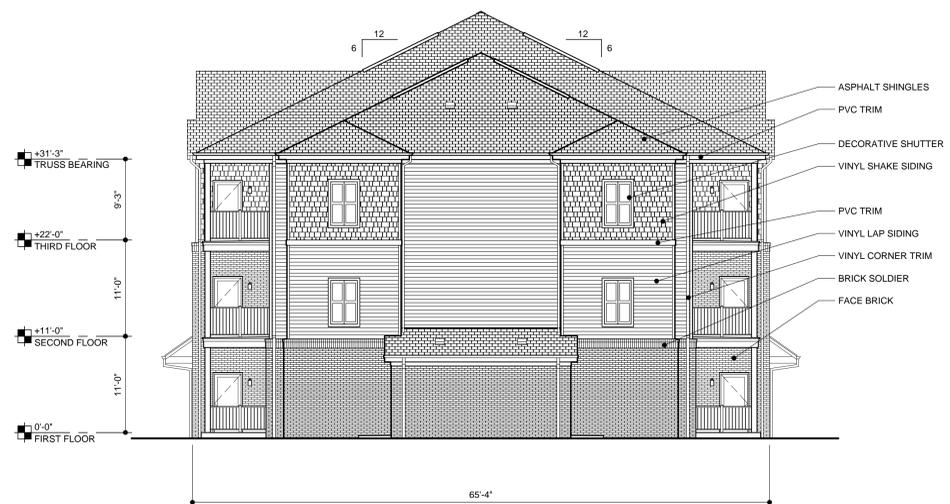
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	PROJECT NUMBER TS316041.00	CONTRACT DRAWING DATE JUNE 17, 2016
CONSULTANTS	REVISIONS / SUBMISSIONS	DRAWING TITLE BUILDING TYPE D - ELEVATIONS
DRAWING NUMBER AD201		SHEET 1 OF 11

M
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F
E
D
C
B
A



- ASPHALT SHINGLES
- PVC TRIM
- VINYL SHAKE SIDING
- PVC TRIM
- VINYL LAP SIDING
- VINYL CORNER TRIM
- BRICK SOLDIER
- FACE BRICK

F1 BUILDING TYPE E - EXTERIOR ELEVATION
 AE201 SCALE: 1/8" = 1'-0"



- ASPHALT SHINGLES
- PVC TRIM
- DECORATIVE SHUTTER
- VINYL SHAKE SIDING
- PVC TRIM
- VINYL LAP SIDING
- VINYL CORNER TRIM
- BRICK SOLDIER
- FACE BRICK

C1 BUILDING TYPE E - EXTERIOR ELEVATION
 AE201 SCALE: 1/8" = 1'-0"

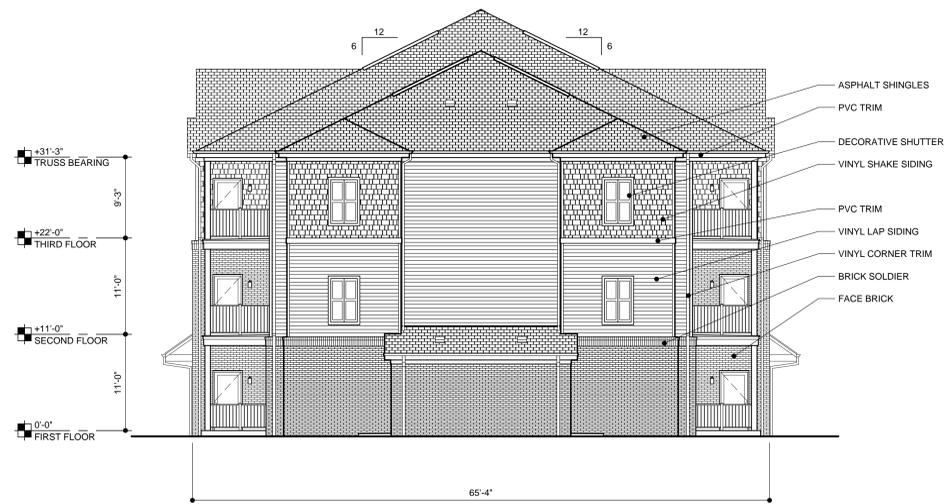
	1789 LONDON BRIDGE ROAD SUITE 201 VIRGINIA BEACH, VIRGINIA 23463 www.ts3architects.com 757.689.2699	PROJECT TITLE THE HAMPTONS PHASE II
	VIRGINIA CORPORATION NO. 406001837	PROJECT NUMBER TS316041.00
CONSULTANTS	REVISIONS / SUBMISSIONS	CONTRACT DRAWING DATE JUNE 17, 2016
		DRAWING TITLE BUILDING TYPE E - ELEVATIONS
		DRAWING NUMBER AE201
		SHEET 1 OF 11

M
G
F
E
D
C
B
A



- ASPHALT SHINGLES
- PVC TRIM
- VINYL SHAKE SIDING
- PVC TRIM
- VINYL LAP SIDING
- VINYL CORNER TRIM
- BRICK SOLDIER
- FACE BRICK

F1 BUILDING TYPE F - EXTERIOR ELEVATION
AF201 SCALE: 1/8" = 1'-0"



- ASPHALT SHINGLES
- PVC TRIM
- DECORATIVE SHUTTER
- VINYL SHAKE SIDING
- PVC TRIM
- VINYL LAP SIDING
- VINYL CORNER TRIM
- BRICK SOLDIER
- FACE BRICK

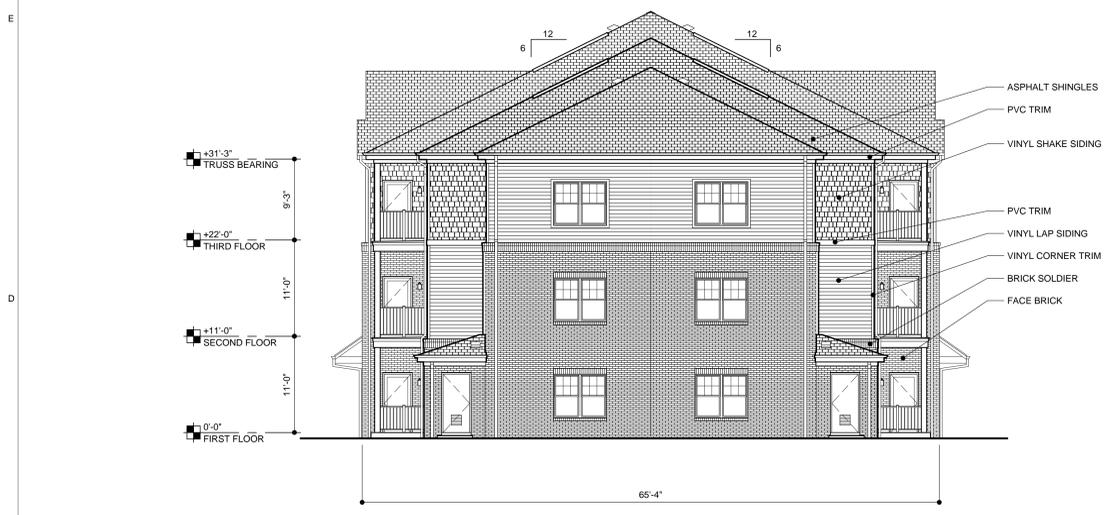
C1 BUILDING TYPE F - EXTERIOR ELEVATION
AF201 SCALE: 1/8" = 1'-0"

	1789 LONDON BRIDGE ROAD SUITE 201 VIRGINIA BEACH, VIRGINIA 23463 www.ts3architects.com 757.689.2699	PROJECT TITLE THE HAMPTONS PHASE II
	VIRGINIA CORPORATION NO. 406001837	PROJECT NUMBER TS316041.00
CONSULTANTS	REVISIONS / SUBMISSIONS	CONTRACT DRAWING DATE JUNE 17, 2016
		DRAWING TITLE BUILDING TYPE F - ELEVATIONS
		DRAWING NUMBER AF201
		SHEET 1 OF 11



- ASPHALT SHINGLES
- PVC TRIM
- VINYL SHAKE SIDING
- PVC TRIM
- VINYL LAP SIDING
- VINYL CORNER TRIM
- BRICK SOLDIER
- FACE BRICK

F1 BUILDING TYPE H - EXTERIOR ELEVATION
 AH201 SCALE: 1/8" = 1'-0"



- ASPHALT SHINGLES
- PVC TRIM
- VINYL SHAKE SIDING
- PVC TRIM
- VINYL LAP SIDING
- VINYL CORNER TRIM
- BRICK SOLDIER
- FACE BRICK

C1 BUILDING TYPE H - EXTERIOR ELEVATION
 AH201 SCALE: 1/8" = 1'-0"

	1789 LONDON BRIDGE ROAD SUITE 201 VIRGINIA BEACH, VIRGINIA 23463 www.ts3architects.com 757.689.2899	PROJECT TITLE THE HAMPTONS PHASE II
	VIRGINIA CORPORATION NO. 406001837	PROJECT NUMBER TS316041.00
CONSULTANTS	REVISIONS / SUBMISSIONS	CONTRACT DRAWING DATE JUNE 17, 2016
		DRAWING TITLE BUILDING TYPE H - ELEVATIONS
		DRAWING NUMBER AH201
		SHEET 1 OF 11



The Hamptons- Phase II
Fredericksburg, VA
06.17.16



The Hamptons- Phase II
Fredericksburg, VA
06.17.16



HUBER MOTOR CARS

October 27, 2016

Honorable Mayor Katherine Greenlaw
City of Fredericksburg, Virginia
PO Box 7447
Fredericksburg, VA 22404

Re: Support for RZ2016-03-Hamptons at Family, L.P.
Hamptons Phase II, Mixed Use Project

Mayor Greenlaw,

I hope this finds you well. The purpose of this letter to express my support for the above referenced rezoning case. As you may be aware, we have been working with the applicant to develop a potential third car dealership adjacent to our Huber Volvo location. We are excited about the potential for this new dealership, and the City's approval of this application will be beneficial to this cause.

I thank you for your time and attention to this matter and happy to further discuss at your convenience.

Very truly,

Clay Huber
President
Huber Motor Cars

cc: Hamptons at Family, L.P.

8322957-1 013680.02753

"Driven to a Higher Standard"





POST OFFICE BOX 30844
ROCHESTER, NEW YORK 14603-0844

November 3, 2016

Honorable Mayor Katherine Greenlaw
City of Fredericksburg, Virginia
PO Box 7447
Fredericksburg, VA 22404

Re: Support for RZ2016-03-Hamptons at Family, L.P.
Hamptons Phase II, Mixed Use Project

Mayor Greenlaw,

I hope this finds you well. The purpose of this letter is to express our support for the above referenced rezoning case. Notwithstanding our success in attracting shoppers and consumers to our Fredericksburg location, we believe having more rooftops in the immediate area will further expand positive opportunities for our store. Thus, we are supportive of this application and believe it will contribute to the short and long term benefit of our investment in the City.

I thank you for your time and attention to this matter and happy to further discuss at your convenience.

Very truly,

Dan Aken
Director of Real Estate and Site Development
Wegmans Food Markets, Inc.

cc: Hamptons at Family, L.P.

Honorable Mayor Katherine Greenlaw
City of Fredericksburg, Virginia
PO Box 7447
Fredericksburg, VA 22404

Re: Support for RZ2016-03-Hamptons at Family, L.P.
Hamptons Phase II, Mixed Use Project

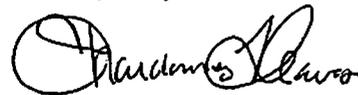
Mayor Greenlaw,

The purpose of this letter is to express my support for the above referenced rezoning case. I work in the immediate area as a Physical Therapist Assistant and desire to live in the City, especially in a community with quality and affordable housing. I believe the Hamptons Phase II project will provide me this opportunity.

I also like the location of this project, including its immediate accessibility to Wegmans and Central Park shops and restaurants. I am very excited about the opportunity to live there and again support this project.

Thank you for your time and attention to this matter.

Very truly,



Shardonney Reaves

cc: Hamptons at Family, L.P.

8341933-1 013680.02753

Honorable Mayor Katherine Greenlaw
City of Fredericksburg, Virginia
PO Box 7447
Fredericksburg, VA 22404

Re: Support for RZ2016-03-Hamptons at Family, L.P.
Hamptons Phase II, Mixed Use Project

Mayor Greenlaw,

The purpose of this letter is to express my support for the above referenced rezoning case. I work in the immediate area as a Customer Service Representative for GEICO and desire to live in the City, especially in a community with quality and affordable housing. I believe the Hamptons Phase II project will provide me this opportunity.

I also like the location of this project, including its immediate accessibility to Wegmans and Central Park shops and restaurants. I am very excited about the opportunity to live there and again support this project.

Thank you for your time and attention to this matter.

Very truly,

LeNeir Hill

LeNeir Hill

cc: Hamptons at Family, L.P.

8341933-1 013680.02753



November 16, 2016

Timothy J. Baroody
City Manager
City of Fredericksburg
P.O. Box 7447
Fredericksburg, VA 22404-7447

**Re: Commitment to City of Fredericksburg ("City")
Low Income Housing Initiatives**

Mr. Baroody,

We hope this letter finds you well. As you are aware, S.L. Nusbaum Realty Co. ("Nusbaum"), which is affiliated with both the Hamptons at Noble, L.P (which owns the Valor a 128 multifamily project currently under construction in the City) and the Hamptons at Family, L.P., which currently has a pending rezoning application before the City to develop a mixed use residential and commercial development project off of Fall Hill Avenue. Nusbaum has been in the residential and commercial development business for over 100 years and is considered an industry leader in developing, constructing and managing multifamily housing and commercial properties. In this regard, Nusbaum has developed over 7,000 apartment units and currently manages approximately 20,000 apartment units in the Southeastern United States.

We are very proud of the accomplishments of our company, including our success in delivering affordable housing to working families in the communities in which we operate. However, our investment in communities includes more than brick and mortar. In this regard, we believe in creating long term relationships with the communities in which we serve, including investments in community priorities and initiatives.

For purpose of the foregoing, we understand that the City is undertaking efforts to address low income housing challenges. Nusbaum is committed to assisting the city with these challenges. Therefore, Nusbaum commits that within no later than 180 days after the rezoning approval for the Hamptons Phase II (RZ2016-03), it will donate to the City \$100,000, which shall be utilized by the City, pursuant to its sole discretion, for low income housing initiatives.

In closing, we are supportive of this community initiative and look forward to working with the City to assist it in accomplishing its low income housing goals.

Very truly,



Miles Leon
President